

# FINAL REPORT

## BLUEPRINT CUMBERLAND II



MAY 2007



## TABLE OF CONTENTS

	<u>Page</u>
Executive Summary .....	3
Introduction .....	18
Blueprint Cumberland I: Issues and Opportunities. ....	21
Existing Studies Issues Analysis .....	27
Blueprint Cumberland II Visioning .....	80
Blueprint Cumberland II Challenges .....	81
Steps for Master Planning – Case Studies .....	89
Master Planning Process .....	110
Draft Master Plan Scenarios .....	118
Recommended Master Plan .....	126
Review of Planning Tools by Peer Communities and Business Districts .....	140
Maintenance Plan Alternatives .....	184
Five-Year Action Plan .....	187
Stakeholders Group and Technical Group .....	190
Conclusion and Recommended Next Steps .....	203

## **EXECUTIVE SUMMARY**

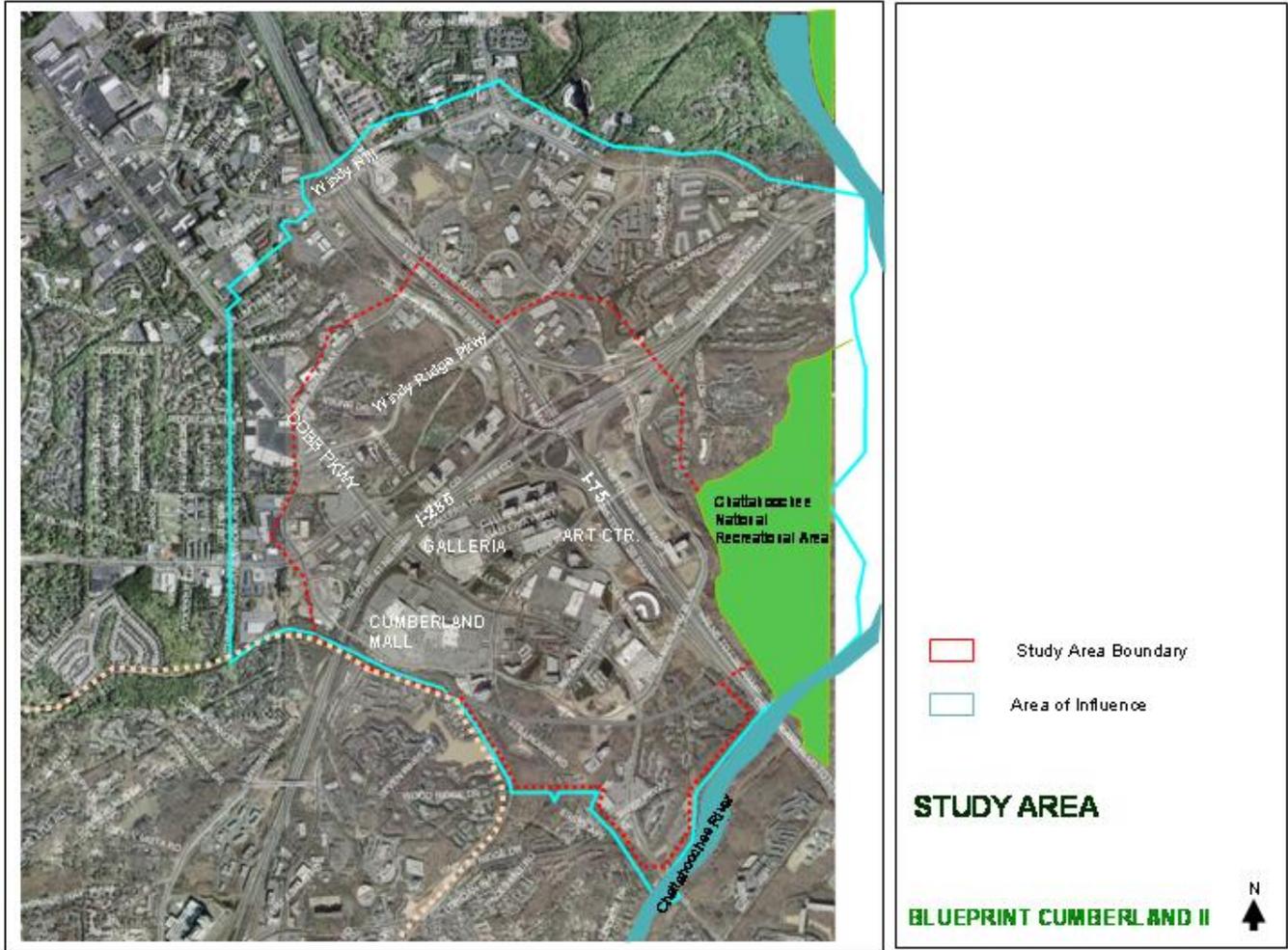
### **The Context**

The metropolitan Atlanta region finds itself at a time when compact development, town centers, pedestrian enhancements and local destinations are becoming a necessary part of our future if we are to maintain and improve our environment and culture. Counties such as Cobb County and many cities in the region are altering their land uses, creating sustainable growth opportunities and a new definition of quality of life. Simultaneously, new policy directions have been articulated by the Atlanta Regional Commission (ARC) and the Georgia Department of Community Affairs (DCA).

Recognizing this trend, the Cumberland CID took the initiative to formulate a blueprint for the future of the Cumberland CID area through an LCI Study – Blueprint Cumberland Strategic Plan in 2001. This LCI study laid ground for a multitude of strategies that addressed land use, transportation, urban design and housing issues. Since then, several initiatives have been successfully implemented. The intent of the Blueprint Cumberland II Master Plan is to determine where the CCID finds itself today in that context, the projects initiated, their effect and how these initiatives have fulfilled the goals of the CCID. The intent of Blueprint Cumberland II is to update this plan to formulate/reformulate the goals based on the current scenario and market trends and develop a strategic plan to address the reformulated goals and the initiatives that are unfulfilled thus far.

The Cumberland-Galleria area is an ‘urban edge-city’ with a multitude of activities and mix of uses; office, retail/commercial, civic, recreational, cultural, convention and residential. It is one of the two largest office markets in the metro area with significant employment. It continues to experience tremendous growth; however, this area has significant land that can be utilized. It is the intent of Blueprint Cumberland II study to set the stage to shape the future growth of this area to ensure that the development patterns will enhance the area, the mix of uses and livable character that can be sustainable. In effect, Blueprint Cumberland II seeks a balanced growth that would benefit the community and ensure prosperity for its coming generations.

**Area of Study**



The Blueprint Cumberland area is a very broad and diverse geographic area comprised of a blending of residential, commercial and natural areas. For the purposes of Blueprint Cumberland II, it was important to distinguish areas of “influence” from the areas of detailed study (Core Area). Therefore, the study area has been divided into the Core Area and Area of Influence as shown in the map above.

1. Blueprint Cumberland Core Area:

The Core Area represents a distinct geographic area that has been the subject of the initial and the current master planning, visioning, and is the subject of a detailed level of study and recommendations. The Core Area is envisioned as the “heart” of the Cumberland CID and will continue to be the most urbanized location within the County.

2. Blueprint Cumberland Area of Influence:

This represents the broadest geographic area of study. In particular, the Area of Influence provides a voice in the planning process for the adjacent stakeholders, within and outside the Cumberland CID, that will be most affected by the plan.

3. Cumberland Community Improvement District

The third area of the Blueprint Cumberland II planning study directly coincides with the boundaries of the Cumberland Community Improvement District (CCID). This is the broadest area of the three and offers the greatest potential for development and redevelopment and expansion, regional planning and regional connectivity.

## **Stakeholder Involvement**

The Stakeholders Group and Technical Team members met over the duration of the Blueprint Cumberland II project with consulting team members in an effort to combine opinions and knowledge to produce a consensus plan that is beneficial to all those involved. Numerous meetings throughout the project were held and valuable information gained by the willingness of the Stakeholders Group and Technical Team members to share opinions and expertise.

The Stakeholders Group included representatives from neighborhood groups, business associations, property owners, development interests, professionals in real estate planning, government leaders, and representatives of other related groups. The Technical Team members were planners and transportation professionals from Cobb County, the cities of Marietta and Smyrna, and the Atlanta Regional Commission.

## **Visioning**

Stakeholders provided direction for setting the strategic vision for the Master Plan. During this process, stakeholders focused on current strengths and weaknesses in the Core Area and offered opportunities for the future.

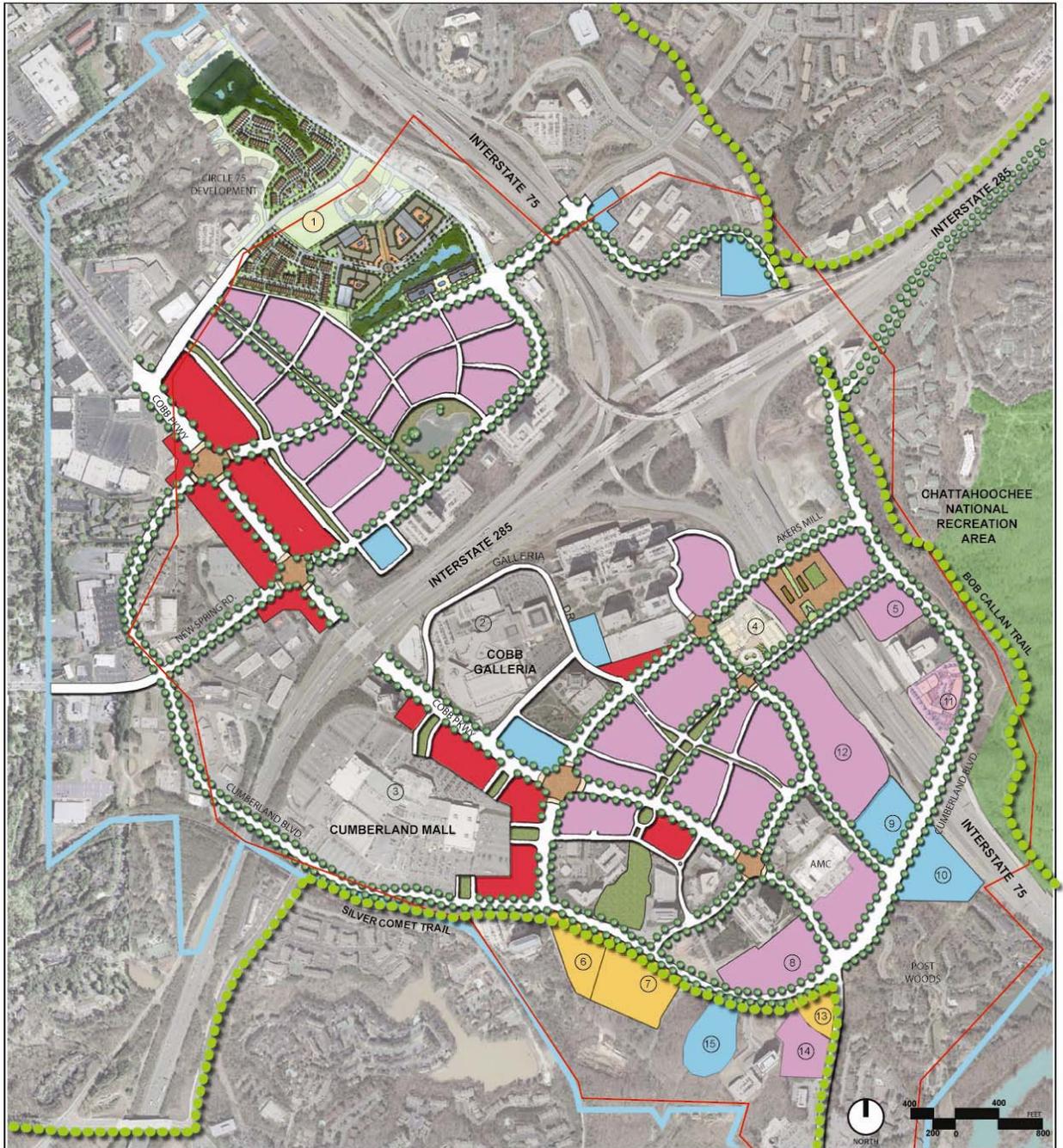
The Cumberland CID's Core Area has several identified advantages including its location, access, strong leadership, superior infrastructure, low taxes, redevelopment potential and its adjacent greenspaces and planned streetscapes and trails. However, stakeholders mentioned several challenges and threats facing the master planning process: aging rental properties, lack of adequate neighborhood shopping opportunities, school system is inadequate in the area, jobs-to-housing ratio, lack of transportation options, a congested interstate system, Sandy Springs expansion, future of Dobbins AFB, perception of area, image of Cumberland Mall, highway expansion, and perception of crime.

The Master Plan should consider several factors according to the stakeholders involved in Blueprint Cumberland II planning process. Among the future issues are the need for neighborhood churches, stores and schools; affordable housing; senior housing; mixed-use development; parks and recreation and more greenspace; shuttle service within the area; a strong marketing campaign; a sense of community; and, the development of a town center or focal point.

### **Blueprint Cumberland II Master Planning**

The Master Plan was developed through initial stakeholder input, review and visioning, an analysis of past studies, issues identification, and a master plan workshop. Based on the community's vision for the Core Area, analysis of existing conditions, an identification of development opportunities and market demand in the area, the planning team prepared several concepts designed to leverage the resources of the Core Area, encourage development and redevelopment and promote a variety of uses including retail, entertainment and residential uses, currently dominated by office uses.

Within the Core Area, there are about 470 residential units and 30,279 jobs. With a jobs-to-housing balance of 64:1 (64 jobs per 1 housing unit), there is a significant deficit in the number of housing units. This reflects the need for additional housing to support employment within the Core Area. However, there have been a number of developments approved through site plan approval and/or zoning approval that have dramatically improved the jobs-to-housing ratio. The Cumberland CID has made outstanding progress in this area as a result of its Blueprint Cumberland planning initiative. The revised jobs-to-housing ratio is displayed in the table below. By including the proposed developments, the ratio improves from 64:1 to 8:1. An ideal ratio for the Cumberland CID would be 4 jobs for every housing unit. Considering that the total



LEGEND		PROPOSED DEVELOPMENTS		
<span style="color: red;">■</span>	RETAIL/COMMERCIAL	1. - CIRCLE 75 ( MIXED USE )	5. - CONDOMINIUMS	10. - OFFICE
<span style="color: lightblue;">■</span>	OFFICE	2. - REDEVELOPMENT MIXED USE ( RETAIL, HOTEL, DWELLING UNITS)	6. - CONDOMINIUMS	11. - HOTEL
<span style="color: pink;">■</span>	MIXED USE	3. - REDEVELOPMENT- ( NEW LIFESTYLE CENTRE)	7. - RESIDENTIAL	12. - GROVE STREET MIXED USE
<span style="color: yellow;">■</span>	MULTI FAMILY RESIDENTIAL	4. - NEW PERFORMING ARTS CENTRE	8. - MIXED USE ( OFFICE, RETAIL)	13. - CONDOMINIUMS
<span style="color: brown;">■</span>	PAVED PLAZAS		9. - OFFICE	14. - MIXED USE
<span style="color: green;">■</span>	TRANSIT STATION			15. - OFFICE



### BLUEPRINT CUMBERLAND II



**PROPOSED DEVELOPMENTS APPROVED THROUGH ZONING AND/OR SITE PLAN APPROVAL PROCESS**

developable land area is 345 acres or 29.4% of the available 1,174 total acres within the Cumberland/Galleria Core Area, this ideal ratio is attainable.

Once the premise of the Master Plan was established, the planning team identified developable/redevelopable land within the Cumberland/Galleria area. Redevelopment areas, other than available vacant parcels, were defined as single-storied strip retail; surface parking lots; aged office/retail and apartment buildings; or other incompatible uses in the area (auto repair shops, etc).

The vision behind the Blueprint Cumberland II is to transform the Cumberland/Galleria area from office/retail, that generates only daytime activity, to a 24/7 lifestyle activity center that supports a live-work-play environment. To create a city center feel for the Cumberland CID with an array of mixed-uses; retail, office, entertainment, apartments, condos, townhomes, detached and attached single-family, the Master plan lays out direction and means to achieve this goal. Any plan or vision of this nature is not expected to be realized overnight or by a single entity. It is a long-term collaborative effort that involves Cobb County, the Cumberland CID, property owners, the development community, agencies and other stakeholders. While these recommendations are expected to be realized in the long-term where the future conditions may change, the Master Plan serves as a blueprint and a guideline for development.

The Cumberland CID Area would benefit from a master plan that promotes a distinctive architectural character and a vibrant discernable activity center - a "focal point." The Master Plan creates a new focal point in the Core Area with a linear town green/public plaza and space for public passive and active recreation, along with an array of mixed-uses: retail, office, entertainment, apartments, condos and townhomes surrounding it. This new activity center will create a sense of place and character for the community. This linear activity center will also connect the existing activity nodes and centers within

the area: Cobb Energy Performing Arts Centre, the Chattahoochee National Recreation Area, Cumberland Mall and the Cobb Galleria. Apart from these prime anchors, the Master Plan also integrates within itself the proposed development projects and infrastructure initiatives.

To promote a more walkable environment in the Core Area, the Master Plan proposes additional vehicular and pedestrian connections. A finer-grained network of pedestrian-friendly streets with wide sidewalks, signage and street trees would provide multiple routes to various destinations within the area along with enhancing the urban environment. Active retail uses along existing and proposed streets, such as restaurants, cafes, shops and boutiques, will further transform the streets into vibrant public spaces buzzing with pedestrian activity. These pedestrian-oriented streets will facilitate the separation of the current superblocks into smaller walkable blocks/development pods. These development pods will be punctuated with a series of interconnected greenspaces and pocket parks to provide relief and passive recreational spaces in a dense urban environment. Smaller blocks will also allow for flexibility in the density and uses that can be incrementally developed in phases.

To enhance accessibility, the Core Area also needs to offer other transportation choices to “office goers,” residents and the visitors living/traveling in the area. The Master Plan proposes a public plaza integrated with the transit station that spans over I-75 and connects the Core Area to the east of I-75 at Cumberland. The area around the transit station is proposed to be developed into a Transit-Oriented Development (TOD) with higher density and diverse uses and activities. The Master Plan also recommends other alternate means of transportation such as multi-use trails and bike paths to further enhance the accessibility/connectivity to and within the Cumberland/Galleria area. Along with incorporating proposed traffic initiatives, including traffic signals, new streets and street alignments, median breaks and improved crosswalks, the plan also identifies

several traffic calming measures such as landscaped medians, paved intersections and pedestrian countdown signals, along the corridor to provide a safe pedestrian environment.

The area north of I-285 and Windy Ridge Parkway, as shown on the Master Plan, consolidates retail along Cobb Parkway with buildings lining Cobb Parkway and parking located in the rear. Pedestrian-friendly streetscapes being designed for Cobb Parkway will enhance this corridor, both visually and functionally. The remaining area in the northern core is proposed to be mixed-use that is predominantly residential supported by neighborhood-based retail. Residential uses would include various product types: townhomes, single-family, condos, apartments and live/work units. The mixed-use will be supported by pocket parks and open spaces.

### **Planning Tools**

As a part of the Blueprint Cumberland II planning process, a number of “cutting-edge” planning tools were reviewed. One of the planning tools that has been implemented by some peer communities and business districts throughout the Southeast and elsewhere in the U.S. is called “form-based coding.”

Currently, Cobb County and its municipalities use traditional zoning codes that divide parcels into different zones/districts (residential, commercial, industrial, etc.); list those allowable land uses in each zoning district; and, indicate the density allowed by regulating such items as building heights, floor area ratios, parcel coverage, setbacks, and other dimensions. By comparison, form-based codes regulate the streetscape, form, and the public realm. In general, these alternatives govern building types, building dimensions, parking locations, and façade features. Form-based codes are prescriptive, because they clearly state those building types, streets and civic spaces down to the parcel level, that are desired. Thus, form-based codes allow decision-makers to deal with the details of mixed-use, transportation and other factors.

Another popular planning tool for accommodating special situations, like those in the CCID and Cobb County, is the overlay district. This is a special zone, created by ordinance, which has its own unique, additional standards (or a schedule of items that automatically replace the requirements in the conventional zoning ordinance). One reason the overlay district is popular is that the county can “overlay” a set of standards for the area without a complete rezoning of the county, while maintaining the status quo of zoning elsewhere in the county. The overlay district tool is applied in several ways. The more traditional approach is for the overlay district standards to be optional. The overlay standards are offered to the developer as alternatives to the conventional standards. Incentives might be provided to the developer to opt for the alternative standards (such as shallower setbacks which increase buildable area). Currently, Cobb County has an overlay district that has been applied to several areas throughout Cobb County.

### **Recommended Next Steps**

The following projects and initiatives are recommended for consideration by the Board of the Cumberland Community Improvement District.

#### **First Steps**

- 1. The Blueprint Cumberland II Planning Area, and the current master planning study, should be expanded to cover the area outside of the Core area and within the CCID boundaries.***
- 2. The growth taking place in both the Cumberland CID, the Fulton Perimeter CID, and the City of Sandy Springs necessitates closer cooperation in transportation planning. A joint transportation planning study should be conducted, as an expansion of Blueprint Cumberland II, to determine the long-term transportation needs of the affected area to properly plan for growth and seamless regional transportation networks.***
- 3. Using the results and recommendations of Blueprint Cumberland II, the Cumberland CID should establish an updated five (5) year list of priority projects.***

- 4. *Blueprint Cumberland II recommends a public plaza integrated with the transit station that spans over the I-75 and connects the Core Area to the west with Cumberland Boulevard to the east. The area around the transit station is proposed to be developed into a Transit-Oriented Development (TOD) with higher density and diverse uses and activities. This includes a connection south of the Cobb Energy Performing Arts Centre in front of the transit plaza to link Cumberland Blvd to the west side of I-75.***
- 5. *The CCID needs to conduct a study of recommendation #4 to determine the feasibility of locating the public plaza and street connection (parallel to Akers Mill Road) over I-75.***
- 6. *To further the recommendations of the marketing study, the CCID should continue to aggressively pursue its action plan to promote the Cumberland CID area to the predetermined business and consumer targeted audiences.***
- 7. *Establishing a physical identity for the Cumberland/Galleria community has been noted by the Cumberland CID as a priority concern to the CCID Board and its stakeholders. The Cumberland CID, in cooperation with the Cobb DOT, should identify funding and soon thereafter begin the implementation of a system of street signs, gateway signs and related gateway improvements, and wayfinding signs using the newly adopted Cumberland CID branding and logo.***
- 8. *To facilitate greater mobility for pedestrians within the Cumberland CID, particularly the Core Area, the CCID should create a Pedestrian Mobility Action Plan that includes:***
  - the location of additional pedestrian bridge crossings;***
  - the location and design for pedestrian crosswalks at all intersections and pedestrian countdown signals at key intersections; and***
  - the location of sidewalks in need of repair, replacement or where connectivity is needed between land uses.***

***The Pedestrian Mobility Action Plan will include locations, conceptual designs, and implementation strategies (costs, funding options, responsible parties, etc.) for each element of the plan.***

## Second Steps

9. As large parcels are redeveloped, the Cumberland CID and Cobb DOT should promote the creation of street grids for improved mobility, pedestrian-oriented streetscapes and access management controls within the new development.
10. The Cumberland/Galleria Area would benefit from a master plan that promotes a distinctive architectural character and a vibrant discernable activity center - a *“focal point.”* Blueprint Cumberland II recommends a new focal point in the Cumberland/Galleria Core area with a linear town green/public plaza and space for public passive and active recreation with an array of mixed-uses; retail, office, entertainment, apartments, condos, townhomes surrounding it. This new activity center will create a sense of place and character for the community and will also connect the existing activity nodes and centers within the area; Cobb Energy Performing Arts Centre, Chattahoochee National Recreation Area, Cumberland Mall and the Cobb Galleria.
11. As identified in the review of planning tools used by peer communities and business districts throughout the U.S., the Cumberland CID should work closely with Cobb County Community Development to investigate the application of a form-based overlay district to govern land use and zoning within the CCID’s boundaries.
12. Mobility around the Cumberland CID area continues to be a concern to stakeholders. There continues to be a need for transit services within the Blueprint Cumberland II Core Area.
13. As the Cumberland CID continues to evolve, greater diversity in housing is essential to meet the demands of the marketplace, particularly housing for “empty-nesters” and retiring “baby-boomers.” The CCID should continue to promote development and redevelopment of mixed-use and residential housing to improve the jobs-to-housing balance within the area including a diversity of housing products/types.

### FIVE (5) YEAR ACTION PLAN

Facility	Action Item	From/At	To	Project Type	Total Project Costs	Responsible Party	CID Cost (Local Match)	Funding Program
<b>US 41 (Cobb Parkway)</b>	Extend northbound left turn lane	Hargrove Road	N/A	Intersection Improvement	\$65,000	Cumberland CID	\$13,000	CID/CDOT
	Restripe the I-285 off ramp from 2 left turn lanes and 3 right turn lanes to 3 left turn lanes and 2 right turn lanes	I-285 eastbound off ramp	N/A	Intersection Improvement	\$5,000	Cumberland CID	\$1,000	CID/CDOT
	Add concrete barrier to separate left turn lanes and through lanes between intersections	I-285 westbound off ramp	Spring Road	Intersection Improvement	\$190,000	Cumberland CID	\$38,000	CID/CDOT
<b>Cumberland Boulevard</b>	Add an eastbound through lane, southbound right-turn lane, and restripe northbound approach to provide a separate left turn lane	Akers Mill Road/Stillhouse Road	N/A	Intersection Improvement	\$470,000	Cumberland CID	\$94,000	CID/CDOT
<b>Akers Mill Road</b>	Akers Mill Road West	Cumberland Boulevard	US 41 (Cobb Parkway)	Pedestrian Improvement	\$341,000	Cumberland CID	\$68,200	LCI/CID
	Akers Mill Road East	West of I-75	Cumberland Boulevard	Pedestrian Improvement	\$247,000	Cumberland CID	\$49,400	LCI/CID
	Akers Mill Road Pedestrian Bridge	Akers Mill Road/ Cumberland Boulevard	Walton River/ Potential Future Residential Redevelopment	Pedestrian Improvement	\$621,000	Cumberland CID	\$124,200	LCI/CID
<b>Akers Mill Road, Mill Green Parkway, Interstate North Circle/Parkway</b>	Akers Mill/ Interstate North	Akers Mill Road/ Cumberland Boulevard	Windy Ridge Parkway	Pedestrian Improvement	\$859,000	Cumberland CID	\$171,800	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard West	Spring Road	Akers Mill Road	Pedestrian Improvement	\$880,000	Cumberland CID	\$176,000	LCI/CID

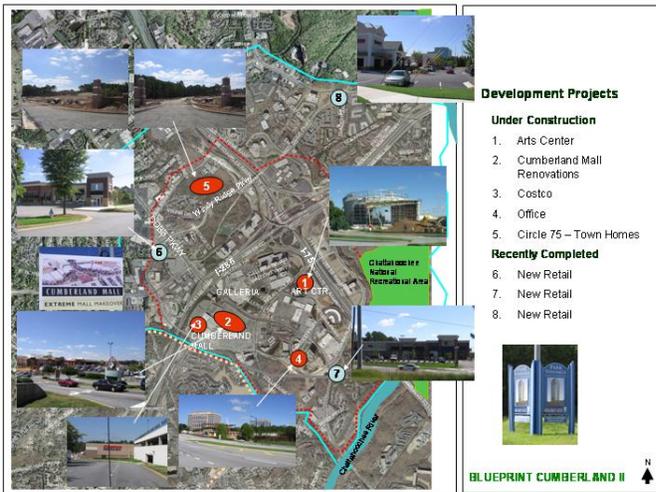
<b>Cumberland Boulevard</b>	Cumberland Boulevard Southeast	Akers Mill Road	US 41 (Cobb Parkway)	Pedestrian Improvement	\$222,000	Cumberland CID	\$44,400	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard Northeast	Overton Park South	Akers Mill Road	Pedestrian Improvement	\$154,000	Cumberland CID	\$30,800	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard South	US 41 (Cobb Parkway)	Overton Park South	Pedestrian Improvement	\$489,000	Cumberland CID	\$97,800	LCI/CID
<b>Cumberland Boulevard and Windy Ridge Parkway</b>	Cumberland Boulevard/ Windy Ridge Parkway	Spring Road	Interstate North Circle	Pedestrian Improvement	\$1,805,000	Cumberland CID	\$361,000	LCI/CID
<b>Circle 75 Parkway</b>	Circle 75 Parkway	US 41 (Cobb Parkway)	Windy Ridge Parkway	Pedestrian Improvement	\$432,000	Cumberland CID	\$86,400	LCI/CID
<b>Spring Road</b>	Spring Road	Cumberland Boulevard	US 41 (Cobb Parkway)	Pedestrian Improvement	\$196,000	Cumberland CID	\$39,200	LCI/CID
<b>Interstate North Circle</b>	Interstate North Circle	Windy Ridge Parkway	Interstate North Parkway	Pedestrian Improvement	\$246,000	Cumberland CID	\$49,200	LCI/CID
<b>Interstate North Parkway</b>	Interstate North Parkway South	Windy Ridge Parkway	Interstate North Circle	Pedestrian Improvement	\$439,000	Cumberland CID	\$87,800	LCI/CID
<b>Interstate North Parkway/ Interstate North Parkway SE</b>	Interstate North Parkway North	Windy Ridge Parkway	Windy Hill Road	Pedestrian Improvement	\$446,000	Cumberland CID	\$89,200	LCI/CID
<b>Overton Park South, Overton Park North, Overton Park Drive</b>	Overton Park Area Pedestrian Circulation Improvements	Overton Park South	Akers Mill Road	Pedestrian Improvement	\$555,000	Cumberland CID	\$111,000	LCI/CID
<b>Riverwood Parkway and Cumberland Center IV/ Homewood Suites Driveway</b>	Riverwood Parkway	Cumberland Boulevard	Cobb Galleria Parkway	Pedestrian Improvement	\$600,000	Cumberland CID	\$120,000	LCI/CID
<b>US 41 (Cobb Parkway)</b>	US 41 (Cobb Parkway) North	Windy Ridge Parkway/ Cumberland Boulevard	Akers Mill Road	Pedestrian Improvement	Funded in TIP/RTP	Cumberland CID	Funded in TIP/RTP	STP Urban >200K
<b>US 41 (Cobb Parkway)</b>	US 41 (Cobb Parkway) South	Akers Mill Road	Cumberland Boulevard	Pedestrian Improvement	\$462,000	Cumberland CID	\$92,400	LCI/CID
<b>US 41 (Cobb Parkway) Pedestrian Bridge</b>	Pedestrian Bridge	Regent/Riverwood Mixed-use Development	Akers Mill Square	Pedestrian Improvement	\$181,000	Cumberland CID	\$36,200	LCI/CID
					<b>\$10,945,000</b>		<b>\$2,189,000</b>	

<b>PLANNING INITIATIVES</b>	<b>Description</b>	<b>Cost</b>	<b>Year</b>	<b>Responsible Party</b>	<b>Funding Source</b>
	Expand Blueprint Cumberland II master planning to include entire CCID area	\$200,000	2007	CCID	CCID
	Joint Corridor Master Plan with CCID, Fulton Perimeter CID and Sandy Springs	\$100,000	2007	CCID	CCID/FPCID
	I-75 Transit Plaza Feasibility and Concept Plan	\$100,000	2007	CCID	CCID
	Marketing Action Plan Implementation	TBD	2007	CCID	CCID
	Design and Implement gateways, signage and related improvements in CCID	TBD	2008	CCID	CCID
	Create a Pedestrian Mobility Action Plan--crosswalks, ped countdown signals, and plan to repair/replace/ and complete connectivity of sidewalks within CCID	\$50,000	2008	CCID	CCID
<b>OTHER INITIATIVES</b>					
	Engage Cobb County Housing Authority to assist in providing affordable and workforce housing	TBD	2007	CCID/Cobb Housing Authority(CHA)	CCID/Cobb Housing Authority
	Assure that the Cobb County Comprehensive Plan Update incorporates affordable and workforce housing elements	TBD	2007	CCID/Cobb County	CCID/Cobb County/CHA
	Engage developers and incentivize the provision of affordable and workforce housing as a part of their projects.	TBD	2007	CCID/CHA	CCID/CHA

# INTRODUCTION

## Background

The Cumberland/Galleria area is a mixed-use, urban edge-city situated in the northwest portion of the Atlanta metro area at the confluence of I-75 (Atlanta's Northwest Expressway) and I-285 (Atlanta's Perimeter Expressway). Encompassing over 24 million square feet of office space, it is one of the two largest office markets in the Atlanta region. Although it is perceived primarily as a commercial market, Cumberland/Galleria is a community comprised of mixed-uses: office, retail and residential.



Over the past 30 years the Cumberland/Galleria area has experienced tremendous growth; however, there is room for additional development. These areas represent a unique opportunity to effectively shape and manage future development patterns so that the entire CCID area will benefit and continue to prosper.

Currently, the Cumberland/Galleria area encompasses approximately 3,100 businesses employing over 125,000 individuals. In addition to the 24 million square feet of office space, the area is served by approximately 10 million square feet of retail space. By 2030 it is forecast that this area will provide employment for over 175,000 individuals, a net gain of approximately 50,000 people. There are also 30,000 residents in 18,000 households that call the Cumberland/Galleria area their home. In 2030, it is estimated that the number of households will increase to 20,000 and the population will be approximately 55,000. The majority of the dwellings are multi-family apartments with other high-density residential communities and single-family homes making up the

balance of the existing residential component. Created by state legislation in 1987, the CCID has taken the lead in identifying, funding and overseeing the implementation of transportation improvements that have sustained and enhanced mobility in the area while at the same time encouraging continued development. Working with Cobb County, the Atlanta Regional Commission, Georgia DOT, and other state and federal agencies, the CCID has leveraged in excess of \$3 billion for transportation and planning-oriented projects since its inception.



County and regional leaders continue to demonstrate an understanding of the challenges to growth and mobility in large activity centers like Cumberland/Galleria and have supported smart growth concepts that seek to address those issues. This is reflected by the fact that the Atlanta

Regional Commission has been awarding planning funds to designated activity centers in the metropolitan Atlanta region. Through the Livable Centers Initiative, these funds are available to examine land-use efficiency, urban design, inter-modal connectivity, traffic demand reduction measures, internal mobility requirements, public and private investment policy, and activity center organization, promotion and restructuring. LCI funding has been made available to implement projects identified in the various studies of activity centers throughout the Atlanta metro area.

Blueprint Cumberland II plans, promotes and implements those land use and transportation elements identified in the Livable Centers Initiative, the ARC Regional Development Plan and the Cobb County Comprehensive Plan and incorporates those elements that are most appropriate for the Cumberland/Galleria area.

## Goals of Blueprint Cumberland II

The primary goals of Blueprint Cumberland II will include, but are not limited to:

- Gathering, organizing and analyzing information regarding public and private initiatives that impact the development dynamics in the study area.
- Identifying the stakeholders' vision for the Cumberland/Galleria community and formulating realistic goals and guidelines that support that vision.
- Developing and implementing programs and projects that assist in achieving the community's stated vision.
- Determining and maintaining an appropriate mix and balance of residential, business and recreational/entertainment uses to ensure the long-term economic viability, market competitiveness and quality of life of the Cumberland/Galleria area.
- Addressing residential development issues particularly as they relate to affordable and mixed income housing needs in the community.
- Providing a complete range of transportation systems and links within the Cumberland/Galleria community that complement existing and planned local, metropolitan and regional systems.
- Creating a well planned, aesthetic, urban environment that enhances the quality of life and promotes safe vehicular and pedestrian movement/circulation.
- Promoting the community's vision, physical improvements and economic opportunities through an effectively targeted promotional and marketing campaign.
- Demonstrating the connectivity between neighboring activity centers in Cobb County, including, but not limited to, Marietta and Town Center.
- Researching design policies and development standards that will promote and encourage the accomplishment of the community vision.
- Coordinating, where appropriate, with neighboring activity centers (i.e. Perimeter, Buckhead, Midtown, etc.) in the Metro Atlanta area.

## **BLUEPRINT I → BLUEPRINT II: ISSUES AND OPPORTUNITIES**

Blueprint Cumberland I analyzed several issues that directly relate to Blueprint Cumberland II. Issues within the general categories of Housing & Employment, Transportation, and Development Opportunities & Design Standards are all core elements in which both studies are founded. Due to the importance of these issues, Blueprint Cumberland I strived to result in a series of comprehensive recommendations for future land use, zoning, development standards, transportation projects and urban design features. These recommendations provide a bridge to Blueprint Cumberland II that spans critical issues and opportunities for the Cumberland CID.

The Blueprint Cumberland I study (2001) and the Supplemental Housing Study (2003) provided a comprehensive assessment of the existing conditions within the study area. These issues are cited in this section of the Blueprint Cumberland II Final Report without editing. It is worth noting that the excellent efforts performed in these and several other studies provide the baseline for the Blueprint Cumberland II Master Plan.

As noted in the Supplemental Housing Study and in the Blueprint Cumberland I report, there is a need to redevelop older apartment units. The studies mention that:

“these complexes are located in a supply-constrained market and therefore still generate significant rents and perform relatively well. This good performance poses a problem for the developer interested in acquiring the property for redevelopment because the substantial rents command an acquisition price that makes redevelopment economically infeasible. The solution to this dilemma and to encourage redevelopment of these rental apartment communities is to increase the density of any redevelopment project on these sites. This increased density lowers the land cost per unit to a level where a private developer can build units affordable to moderate-income families.” (*Supplemental Housing Study*, 2003, p.13)

The Supplemental Housing Study confirms the issues in Blueprint Cumberland I and recommends three mechanisms or approaches that will achieve the desired goal of reducing the jobs: housing balance and the opportunity to redevelop older apartment

complexes. These three recommendations that have been implemented within the Blueprint Cumberland II Master Plan are: inclusionary zoning; conversion of existing units; and, development of new housing projects.

Another critical issue mentioned in Blueprint Cumberland I study was the problem of ‘supergrids’ within the study area. Blueprint Cumberland II carried this point further and also recognized the critical link between transportation and land use driving master planning in the study area. As pointed out in the Blueprint Cumberland I study, “concentrating diverse and dense land uses where infrastructure is most developed not only makes sense in terms of judicious use of public funds, but develops a ‘critical mass’ of population that can support economic activity and additional modes of transportation.” In the past, the area has depended on this ‘superblocks’ with wider roads to move primarily single occupant vehicles (SOVs) between large developments. Since the level of service and congestion were not a critical issue in the past, ‘superblocks’ were manageable within the area’s traffic grid. However, the application of ‘superblocks’ depends on the fact that most land uses being office or retail, because these uses generate traffic during peak hours that is from point-to-point and best facilitated by the quick mobility of arterials roads. As more residential uses are added to the area to meet the CCID’s vision of a more mobile, 24/7 community, the use of the ‘superblock’ becomes less important and is replaced by the need for a network of smaller grid-like road systems that provide easy access and walkability for those that plan to live-work-play in the study area. This fact is best expressed in the Supplemental Housing Study. “For people to choose to move to the core, they require quality and options. Because their leisure time is impacted by housing location, environmental quality is a major factor in housing choice; because diverse and active lifestyles demand mobility, transportation options are essential. To market the core to prospective residents, streets need to function not only as traffic conduits but as vital and engaging public spaces. The great resource of the Chattahoochee National Recreation Area, one of Atlanta’s largest park and open spaces, must be made visible and easily accessible.”

## HOUSING & EMPLOYMENT

- Although there remains a need for research responding to housing preferences, it is likely that many will not have the same suburban or urban housing product preferences as those in larger households with school-age children. There should be a sizeable enough shift to create greater opportunities than in the past for alternative development patterns featuring a broader mix of housing types and densities. This creates an opportunity to serve more of those households with housing closer to employment and services, slowing the rate of growth of regional congestion and enabling more commercial properties to be developed that become increasingly mixed-use in character.
- Cumberland/Galleria is far from being a 24-hour area today, due primarily to the lack of residential development in the Core Area, as well as an aging retail environment, over-abundance of parking and a lack of pedestrian-friendly infrastructure.
- The impact of insufficient housing in the Cumberland/Galleria core, combined with the area's lack of a sense of place, threaten its long-term economic stability. Today, "the Cumberland area is slightly more than 50% built-out, with several large tracts of land in the core itself being vacant or underutilized." (*Supplemental Housing Study*, 2003, p. 3)
- There is an increasing imbalance between jobs and housing in the area. As nearby housing prices escalate, especially for-sale housing, this imbalance causes lower and middle-income households to drive to jobs in the area from increasingly greater distances.
- There is a lack of homeownership resulting in a high proportion of renters.
- An issue is the potential deterioration and decline of portions of the existing housing stock, particularly the abundant stock of aging rental properties on the fringes of the area.
- From a market perspective, Blueprint Cumberland I states that the over-abundance of rental apartments place a strain on existing schools and draw the disfavor of neighboring residential communities. The incomplete retail mix, in terms of both neighborhood retail, and office-supporting retail; and the poor existing urban design of the area inhibit pedestrian activity. If not addressed, these factors will limit future retail, residential and investment opportunities.
- "Approximately 8,900 units in 29 complexes are potential candidates for redevelopment." (*Supplemental Housing Study*, 2003, p. 13)

## TRANSPORTATION

- There is also a lack of connectivity throughout the area, due to both the interstates, limited multimodal options, and lack of a grid network.
- Additional traffic calming studies and a series of traffic calming projects are needed within the Blueprint Cumberland Area.
- Transportation issues and opportunities include the quantity and quality of sidewalk and roadway projects planned for the area, a need to improve coordination between bike, sidewalk and roadway projects, and, the need for additional trail projects and connections.

## DEVELOPMENT OPPORTUNITIES & DESIGN STANDARDS

- Infill areas should be considered for a mixture of residential uses.
- There is a need to identify areas that are ripe for development, including vacant and underutilized parcels, deteriorating structures, and structures needing rehabilitation. Urban design principles must be developed and applied to evaluate how to create a sense of place, promote multimodal connectivity, and promote cultural uses.
- “A reduction in the number of rental apartments on-site to no more than 40% of total units provided.” (*Supplemental Housing Study*, 2003, p. 14)
- “Within the core of Cumberland, amend office zoning to better qualify mixed-use development as an equally opportune development opportunity to single-use office and amend retail/ service areas to allow residential development.” (*Supplemental Housing Study*, 2003, p. 16)
- Outside of the Blueprint Cumberland I Core Area, the major focus shifts to allowing redevelopment of rental apartments to a mix of for-sale and rental housing.
- Noticeably missing in the study area are “public” spaces, which can be anything from small corner parks, to linear parks, to large open spaces meant for gatherings.

- A 6 to 8 acre public park was recommended within the heart of “Downtown” Cumberland/Galleria. This open space would be developed in association with the mixed-use redevelopment of Akers Mill Square. The park would be surrounded by mixed-use development including offices, residential, and storefront retail. The park would include public art, landscaping, lighting, seating, a covered pavilion, shade trees, event space, a clock or bell tower, and a possible interactive water feature. The park’s exact location and configuration will depend upon the eventual development plan for Akers Mill Square, but should have connections to the adjacent cultural and entertainment development site.
- Retail centers “could be anywhere” retail architecture. The retail currently lacks a strong sense of place and the pedestrian orientation being employed in new town village retail concepts that provides shoppers the ability to walk throughout the entire development. Outside of the area, The Avenues at East Cobb has been highly successful in competing with Cumberland Mall for tenants. The Forum on Highway 141 in Norcross is a similar successful village retail development.
- Urban Design issues and opportunities are the transition of land uses (and appropriate buffers) from high-density and commercial areas to low-density residential areas, the character of future development along Cobb Parkway; the impact of additional traffic in residential and neighborhood shopping areas; and, the underutilization of natural amenities such as the Chattahoochee National Recreation Area.
- Currently, the area encompasses eight zoning categories - only one of which includes residential uses. However, the Redevelopment Overlay District (ROD) adopted by Cobb County for US 41 could be applied within the Core Area to facilitate mixed-use development in that area.
- The Blueprint I report states “the Cumberland Activity Center Area functions more as a series of small islands in a sea of parking and asphalt than as a mixed-use integrated urban core.” The report further indicates that “the poor existing urban design of the area inhibits pedestrian activity (favoring the automobile), and does little to draw people into the Blueprint Cumberland Core Area. If not addressed, this would limit future retail, residential and investment opportunities.”
- New development should seek to provide a balance of land uses within walking distance of each other (preferably within the same building – i.e., “vertical mixed-use”). This will be particularly important in areas served by transit.

- “New development should seek to improve internal mobility within sites by allowing for pedestrian throughways, internal streets, etc. Supergrid development should be strongly discouraged, particularly when it interrupts the overall pedestrian circulation system of the area as a whole. In practice, this may be difficult to achieve because it will require some degree of coordination between various developments and will create areas of the site that is open to the public. To the extent possible, redevelopment of large superblocks should seek to dedicate new internal streets back to the public realm to avoid public-private issues of liability.” (*Supplemental Housing Study*, April 2003, p. 69)
- Currently, there are very few opportunities to access the Chattahoochee NRA. Providing additional/improved pedestrian gateways will enhance the area’s viability as a “livable center.” Proposed locations for improvements include the entrance along Cobb Parkway at the river and at various locations along Cumberland Boulevard.
- In addition, design guidelines intended to set minimum standards for pedestrian-based site planning and material quality should be considered for the Blueprint Cumberland Core Area and surrounding areas.

One of the goals of Blueprint Cumberland II was to identify those issues identified within the Blueprint Cumberland I Study that remain to be addressed. Blueprint Cumberland I has served as a valuable “road map” for the Cumberland CID’s future and Blueprint Cumberland II is the bridge from that “road map” to the future.

## EXISTING STUDIES ISSUES ANALYSIS

### Current Cumberland CID Transportation Improvements

PROJECT NAME	PROJECT DESCRIPTION	CURRENT PHASE
<b>PLANNING</b>		
Blueprint Cumberland I	Launched original Strategic Plan which focuses on LCI projects in the Core Area.	ongoing
Blueprint Cumberland II	Updates original plan and recommends next 5 years of projects. Due in Feb.2007.	in process
CH2M Hill	Creating a pilot project to manage stormwater runoff.	in process
SE Quadrant Traffic Study	Project studies traffic needs of mall area over next 20 years	complete
Circulator Study	Study circulator needs for community	in process
Adaptive Traffic Signal Study	Retime signals in district.	
High Speed Rail Study (ATL to Chattanooga)	Studying high speed rail between ATL & Chatt. CCID contributed \$875,000.	in process
<b>TDM/PUBLIC TRANSPORTATION</b>		
Public Transportation	Project funds transit-related projects for CCID/TMA	ongoing
Commuter Club TMA	Project funds TMA operational and marketing needs	ongoing
Travelers Info Website	Online trip planning tool.	in process
Vanpool Subsidy Financing	Project funds subsidies related to vanpool formation and operations	ongoing
<b>BIKE/PED/STREETSCAPE</b>		
<b>Blueprint Cumberland Projects:</b>		
I-285 Interchange Landscape (TE)	Landscape improvements to interchange. TE project.	Final Design
Cobb Pkwy Streetscape/Ped Improvements (LCI)	Phase 1 of Cobb Parkway streetscape project from Akers Mill north to Circle 75. LCI project.	Final Design
Cobb Galleria Parkway Streetscape (LCI)	Streetscape Improvements to road.	ROW & Const Plans
Akers Mill Rd Streetscape (LCI)	LCI project. Only enough money to pay for portion of Cobb Pkwy to I-75.	ROW & Const Plans
ROW appraisals for Cobb Galleria/A Mill Ped Imp	n/a	Underway
Cumberland Pkwy/Paces Ferry Corner Design	Design for potential "gateway" into the Cumberland/Galleria area from Paces Ferry Road.	complete
<b>Sidewalks/Trails:</b>		
Sidewalk Gaps (construction of misc. gaps around area)	Many are complete. Check on current status description	
Bob Callan Creek Multi-Use Trail Ph 1 (Design)	2.1 mile trail from Cobb Pkwy to Interstate North trailhead	Construction
Bob Callan Creek Multi-Use Trail Ph 1 (Const. Match)	See above	Construction
Bob Callan Creek Multi-Use Trail Ph 2 (Design)	2.1 mile trail from Interstate North trailhead to Terrell Mill Rd.	Design Phase
Mt. Wilkinson Multi-Use Bridge over I-285	Multi-Use bike/ped bridge at Mt. Wilkinson Pkwy over I-285. Part of Silver Comet Trail	Pending complete
Interstate North Pkwy Sidewalk / Trail	Extends trail along IN Pkwy from Powers Ferry to NRA @ IN Pkwy	Preliminary Design
Silver Comet Trail extension	Extends trail from S. Atlanta Rd to Paces Ferry to Mt Wilkinson bridge (1.5 mile)	Construction
Spring Rd portion of Multi-Use Trail	Extends trail from Spring Rd to Cumberland Blvd south to CCT station	Design Phase
<b>Other:</b>		
Cumberland Blvd Landscape (design)	Streetscape improvements along Cumberland Blvd. from Cobb Pkwy. to Akers Mill	Complete
Cumberland Blvd Landscape (const)	See above (construction funds)	Design Phase

ROAD PROJECTS		
Cobb Pkwy Widening (two parts: Fulton and Cobb)	Widens Ph. 1) Akers Mill to Paces Mill, Ph 2) bridge to Mt. Paran	Design Phase
Leland Dr Extension	Extends road to Terrell Mill Rd	Design Phase
NE Quadrant Projects	Four intersection improvements from Delk Rd to Old Canton Rd.	underway
Terrell Mill @ Delk (CCID Project # 1008-5)	Improves intersection capacity and safety as well as pedestrian mobility.	complete
Terrell Mill @ Paper Mill (CCID Project # 1008-6)	Improves intersection capacity and safety as well as pedestrian mobility.	Construction
Terrell Mill @ Paper Mill	see above	Construction
Terrell Mill @ Lower Roswell (CCID Project # 1008-7)	Improves intersection capacity and safety as well as pedestrian mobility.	Construction
Lower Roswell @ Old Canton (CCID Project # 1008-8)	Improves intersection capacity and safety as well as pedestrian mobility.	complete
Wildwood Pkwy Extension landscaping/streetlights	Creates road on back end of Wildwood.	Complete
Wildwood Pkwy Extension const. (CCID Project # 1016)	Creates road on back end of Wildwood.	Complete
Windy Hill / Powers Ferry Flyover Bridge	Concept is evolving to remove flyover and wide PF from WWD Pkwy to TM.	Concept Design
Powers Ferry Widening & Windy Hill Widening	Add additional lane to Powers Ferry from WH to Terrell Mill	Concept Design
Windy Hill / I-75 Interchange (design)	Redesign of interchange now a part of the NW transit/HOV/TOL project (~\$80m cost)	Concept Design
Windy Hill / I-75 Interchange (design and future const. needs)	Redesign of interchange now a part of the NW BRT/HOV/TOL project	Concept Design
East-West Connector / I-285	Widen EWC from I-285 to S. Cobb Drive. Phase 1 is Atlanta Rd to S. Cobb Dr.	Construction
Atlanta Rd/I-285 Interchange reconstruction	Reconstruct interchange. Recv'd earmark in SAFETEA-LU as well as Cobb funds.	Concept Phase

## **TRANSPORTATION ELEMENT: COBB COUNTY COMPREHENSIVE PLAN**

An efficient transportation system is crucial to the economic and social well-being of a high growth area such as Cobb County. It is the circulatory system that brings people, products, and services into the county and allows them to move from one activity to another. The Cobb County Board of Commissioners has recently embarked on one of the most ambitious local road improvement programs in the nation. In addition, in 1989 the county established a public transit system. These programs reflect the high priority of transportation to the people of the county.

Land use planning and transportation planning are often interrelated. While the location, type, and density of land use play an important role in determining traffic flow and volumes, it is also true that road network and availability of transit can affect future land use decisions. The Cobb County Comprehensive Plan identifies the Cumberland/Galleria area as a major traffic generator. "Office space, retail space, and hotel/motel rooms have increased rapidly since Cumberland Mall was built in 1973 near the I-75/I-285 interchange. The office/retail density now rivals that of many city "CBDs." There are over 15 million square feet of office and retail space and approximately 2,500 motel/hotel rooms, and IBM and General Electric have offices here."

Cobb's interstates enable movement of large volumes of through traffic. The network's arterials provide for through traffic movement, yet these arterial roads do have restrictions relating to driveways and access roadways due to minimum rights-of-way of 100 feet. Collectors provide for traffic movement between arterial and local streets with direct access to Cobb County's abutting properties. Major (80 ft. R/W) and minor collectors (60 ft. R/W) differ based on the land use density and how the uses relate to the arterial network. County streets direct vehicles to abutting property and move traffic locally within residential areas.

Cobb Community Transit (CCT) began operations in July 1989. Cobb County staff plans, markets and manages CCT service. A private company contracted by Cobb County operates CCT and maintains the buses. CCT provides fixed route bus service linking residential areas to employment, educational, medical and commercial facilities in many parts of the county as well as in Atlanta. In 1997, over 2.5 million passengers were carried on CCT. Two types of fixed route service are provided: Local Service, with buses stopping at numerous destinations along the routes.

Availability and access to transit in Cobb County is provided by Cobb County Community Transit (CCT) and the Georgia Regional Transportation Authority (GRTA). The system provides GRTA express service to downtown and along the I-20 corridor in southern Cobb County. CCT provides local routes, through I-75 and I-20, connecting the six urban communities (Marietta, Smyrna, Austell, Powder Springs, Kennesaw and Acworth) and unincorporated Cobb County with downtown Atlanta and the MARTA transit system. In terms of access to bus service, the majority of the urban areas have bus stops along primary corridors. The system has park-and-ride lots located in Acworth, on Busbee Drive in the Town Center Area CID, in East Cobb at Johnsons Ferry Baptist Church and in Marietta and the main CCT terminal on South Marietta Parkway. CCT operates a transfer station in the Cumberland CID area just behind Cumberland Mall. Major corridors served by transit include US 41/Cobb Parkway, SR 120, South Cobb Drive, Floyd Road/Mableton Pkwy., Windy Hill Road, Atlanta Street, and the Church St. /Bells Ferry Road corridor. Ridership exceeded 3.5 million trips during all of 2004. Several of Cobb County's trail systems are accessible by CCT buses, which provide bus racks for patrons.

A permanent transfer center in the Cumberland Mall area on Cumberland Boulevard. was opened in September 1998 in conjunction with other CCT service changes. The Cumberland Boulevard Transfer Center, located across from Cumberland Mall, provides a place for passengers to wait while transferring to other routes. The transfer center has a variety of passenger amenities including benches and shelters. An additional building is also being constructed as office space for CCT supervisors. The Cobb County Police Department will also utilize this facility.

CCT also provides specialized transportation service to people with disabilities who are unable to use the regular transit system. Curb-to-curb service is provided on a reservation basis using small paratransit buses.

In accordance with the Americans with Disabilities Act of 1990, the service is provided within one and one-half mile corridors centered along local fixed routes. The paratransit service operates within Cobb County only; no service is provided to locations outside of the County.



# Cobb County Comprehensive Transportation Plan

Cobb County, Acworth, Austell, Kennesaw, Marietta, Powder Springs, and Smyrna

Fact Sheet

Winter 2006

Partners In Progress

## Cobb County Comprehensive Transportation Plan to Reflect Community Vision, Goals for County and Its 6 Cities

### INTRODUCTION

Cobb County and its six partner cities have launched a comprehensive planning study of transportation needs and challenges. Beginning with a review of existing conditions and projected future demands, the study team will also assess land use and transportation relationships throughout the county. The study will investigate alternative ways to ensure the efficient movement of people and goods over the next twenty-five years.

The planning study will examine peer county systems and plans in other regions of the United States, conduct multi-modal technical analyses, build and test scenarios involving various land use schemes as well as all modes of transportation. Modes studied will include all systems from roads to pedestrian and bicycle paths to mass transit and freight movement alternatives. Financial options, impact analyses and draft development plans will be studied to arrive at recommendations for the future. All with full public participation and review.

### WHAT ABOUT PREVIOUS STUDIES?

The Study will utilize and build on previous related work done in the County and cities over the last five years or so. Among the documents to be reviewed are the Cobb County Transit Development Plan, County and City Future Land Use Plans, the eight completed Livable Centers Initiative (LCI) studies, the Northwest Connectivity Study, the Station Area Planning Study along I-75 and the ARC TIP and RTP. The approved results of all of these studies and plans will inform the CTP and minimize duplication of effort. The Study efforts and findings will result in an Enhanced County-Wide Multi-Modal Comprehensive Long-Range Transportation Plan (CTP), which will be subjected to detailed technical and financial analyses and community review prior to adoption by the County Commission and City Councils.



### WHERE IS THE STUDY AREA?

The Study Area encompasses the entire county and the municipalities: Acworth, Austell, Kennesaw, Marietta, Powder Springs and Smyrna.

### WHY IS THE STUDY NEEDED?

While the explosive growth experienced in the 70's and 80's has slowed, Cobb County continues to grow at a healthy rate. The population, now 664,000 according to the most recent estimates by the US Census Bureau, is expected to reach or surpass 754,000 by 2030. Much has been done to upgrade the transportation network to more effectively deal with the explosion in growth and much more will be done in the near term to provide early relief from traffic congestion and to improve the safety of the county's transportation network. For example the Special Purpose Local Option Sales Tax (SPLOST) approved by the voters last September will fund dozens of transportation projects that will begin improving mobility in the County and Cities of Cobb right away. But there is a need for a long-range, comprehensive plan that is multi-modal in nature and that is integrally linked to the land use plans that have been developed or are in the process of being developed by the County and the Cities of Cobb. Thus the need for the Comprehensive Transportation Plan that will serve as the blueprint for transportation investments by Cobb County and its municipalities for the next 25 years.

# COBB COUNTY COMPREHENSIVE TRANSPORTATION PLAN

## Vision Statement

COBB COUNTY'S TRANSPORTATION SYSTEM WILL BE A SAFE AND EFFICIENT NETWORK PROVIDING MULTI-MODAL SERVICE TO COORDINATED LAND USES, WITH SIGNIFICANT GREENSPACE, AND "LIVE-WORK-PLAY" COMMUNITIES.

## Measurable Goals and Strategies

### 1. Goal: System Performance and Safety---

#### To Improve the Overall Performance and Safety of the Transportation System.

- ❑ Reduce the amount of time system users have to spend in congested conditions.
- ❑ Reduce ozone levels below standards and reduce overall air pollution.
- ❑ Reduce the number of roadways that are operating at level-of-service F.
- ❑ Maximize use of public transit.
- ❑ Reduce wait times for buses and increase frequencies.
- ❑ Reduce time of overall commuter through trips.
- ❑ Reduce the number of traffic incidents.
- ❑ Reduce the number of pedestrian and bicycle accidents.
- ❑ Increase safety of facilities for non-traditional modes of transportation (helicopters, etc.).
- ❑ Increase lanes on arterials to reduce "cut-through" traffic.

#### Strategies:

- √ Operate CCT seven days a week.
- √ Locate bus stops so as not to impede traffic flow and create safety issues.
- √ Coordinate location of all bus stops—CCT, school, transit.
- √ Establish and enforce more designated truck routes.
- √ Improve method of monitoring traffic flow.
- √ Establish access management guidelines.
- √ Increase the number of road and railroad grade separated intersections.
- √ Develop strategies for keeping people off roads/traffic demand management—  
increase telecommuting and use of flextime.
- √ Expand traffic light synchronization to non-peak hours.
- √ Install lighting on trail system.

## 2. Goal: Multi-Modal System---

### To Develop a Multi-Modal System in which Each Mode is Optimized Providing the Citizens of Cobb County with Attractive and Realistic Travel Alternatives

- ❑ Increase bicycle connections to activity centers.
- ❑ Increase transit system to serve both local circulator trips and long haul trips.
- ❑ Increase and upgrade pedestrian/bike facilities.
- ❑ Increase dependence on commuter rail and light rail service.
- ❑ Increase use of circulator service within activity centers.
- ❑ Increase the development and use of alternative fuel vehicles.
- ❑ Increase alternatives to the use of a vehicle without owning it.
- ❑ Increase multi-modal use by 100% by 2030.
- ❑ Reduce travel delays caused by multi-modal traffic.
- ❑ Reduce/moderate increases in annual VMT.
- ❑ Increase flexibility of the transportation system to serve all members of the community.

#### Strategies:

- √ Link public transit stops to trail system.
- √ Provide incentives for transit use.
- √ Operate CCT seven day a week.
- √ Provide limited stop cross-county rail service.
- √ Develop a second (smaller) regional airport as an alternative to Hartsfield-Jackson.

## 3. Goal: Land Use Connectivity---

### To Develop a Transportation System that is Appropriate to the Land Uses it Serves

- ❑ Increase the capacity to handle anticipated land use.
- ❑ Increase mixed-use developments that optimize both the use of land and transportation resources.
- ❑ Increase greenspace set asides within new developments and increase greenways.
- ❑ Increase higher density and mixed-use developments along major transportation corridors thereby maximizing use of transportation resources.
- ❑ Increase transportation facilities that will encourage mixed-use developments.
- ❑ Increase conversion rate for obsolete retail to high-density mixed-use development along major transportation corridors.

Strategies:

- √ Connect activity centers via transit routes.
- √ Include a transit component in all development plans.
- √ Enforce development restrictions to limited access facilities.
- √ Coordinate land use, parking, pedestrian and bicycle access and transit access.
- √ Promote/ensure jurisdictional coordination for land use planning.

**4. Goal: Financial Management---**

**To Ensure that Adequate Financial Resources are Available to Maintain Properly the Transportation System and to Grow and Adapt the System in Keeping with Changes in Land Uses**

- Increase the availability of funds to make improvements in the transportation system.
- Increase use of public-private partnerships to fund transportation improvements.
- Implement countywide impact fees to fund transit and road improvements.
- Increase investment to a level that will permit bringing all roads to at least a level-of-service D.
- Increase funding so that there is a truly regional mass transit system by 2030.

Strategies:

- √ Continue use of SPLOST as a means to fund long- and short-term transportation improvements.
- √ Secure state and federal funds for rail transit development.
- √ Establish toll roads to fund road improvements.
- √ Establish a fund for sidewalk improvements, bike paths, greenspace, etc.
- √ Seek multi-county funding for regional road projects.
- √ Improve coordination of funding resources between programs, such as CCT and school transportation.

## **Cobb County Comprehensive Transportation Plan Corridor Needs Assessments**

The overall mobility in Cobb County is dependent upon the performance of certain critical roadways throughout the County. Based upon their existing and projected share of the County's overall traffic and the level of existing and anticipated development along them, the following key corridors were included in this analysis:

- Canton Road
- East-West Connector
- Barrett Parkway-Ridgeway Road-  
West Cobb Loop
- Cobb Parkway (US 41)
- Veterans Memorial Highway (US  
78)
- SR 120 (Dallas Highway)
- SR 120 (Roswell Road)
- US 278 (Thornton Road)
- SR 280 (South Cobb Drive)
- SR 176 (Mars Hill Road)
- SR 92 (Cherokee Road)
- SR 360 (Macland Road)
- Stilesboro Road
- Powder Springs Road
- Johnson's Ferry Road

The following conditions were inventoried for roadways in order to identify specific needs within each of the corridors listed above:

- Roadway Characteristics – roadway conditions based on GDOT standards and functional classification;
- Traffic Conditions, Intersection, and Safety – traffic characteristics including daily traffic volumes, travel time, levels of congestion, and accident locations;
- Bicycle and Pedestrian Facilities – a general overview of bicycle and pedestrian travel characteristics;
- Transit – an overview of the transit services being provided in the corridor;
- Freight – information regarding the truck designation and truck traffic characteristics along the corridor;
- Land Use and Community Resources – an overview of the development type and community resources along the corridor;
- Access Management – information regarding the ingress and egress of surrounding development along the roadway; and
- Future Roadway Improvements – a list of planned and programmed improvements within the Cobb County Special Purpose Local Option Sales Tax (SPLOST) and the Atlanta Regional Commission (ARC) Transportation Improvement Program (TIP) and, if applicable, other planned improvements by the Georgia Department of Transportation (GDOT).

For the Blueprint Cumberland II report, only those corridor needs assessments that impact the study area are included.

## **Cobb Parkway (US 41) Corridor**

### Corridor Description

The Cobb Parkway (US 41) Corridor is a four-lane arterial that begins at Spring Road near I-285 and extends northwest to Bartow County. US 41 is the primary north-south arterial within Cobb County and serves a high number of through trips in addition to local trips. As such, the roadway traverses multiple municipalities - Acworth, Kennesaw, Marietta, and Smyrna. In addition to direct accessibility to I-285, the corridor is very accessible to I-75 via numerous intersecting roadways.

### Roadway Characteristics

The functional classifications assigned for roadways within the corridor are provided in the table below.

### **Functional Classification of Roadways – Cobb Parkway (US 41) Corridor**

<b>Arterial</b>	<b>Major Collector</b>	<b>Minor Collector</b>
Dallas Acworth Highway	<b>Cedarcrest Road</b>	Kemp Drive
Lake Acworth Drive	<b>Mack Dobbs Road</b>	Jim Owens Road
Mars Hill Road	<b>Roberts Road</b>	Airport Road
Nance Road	<b>Franklin Road</b>	Gresham Road
Acworth Due West		
Pine Mountain Road		
Kennesaw Due West		
Chastain Road		
Jiles Road		
Barrett Parkway		
Old U.S. 41		
Bells Ferry Road		
Allgood Road		
North Marietta Parkway		
Roswell Street		
South Marietta Parkway		
Terrell Mill Road		
Windy Hill Road		
Spring Road		

## Traffic Conditions, Intersections, and Safety

There are forty-five (45) signalized intersections along the US 41 Corridor and five (5) traffic regulatory cameras are positioned between Barrett Parkway and Spring Road. Level of service (LOS) is a letter designation used to describe traffic operating conditions, on a declining scale from A to F. LOS A represents free-flow traffic conditions and LOS F represents extreme delays with stopped traffic conditions. Service flow at LOS E is the value that corresponds to the flow rate being at the full design capacity of the facility. Roadway congestion along the segments of the US 41 Corridor in 2005 is presented in the table below.

## Pedestrian and Bicycle Facilities

Sidewalks exist along much of the US 41 Corridor. There are also sidewalks along some portions of the collectors and arterials that intersect US 41. However, the corridor is void of bike paths and effective sidewalk connections. The high volumes of vehicular traffic and resulting congestion during peak travel times create a less safe environment for pedestrians and bicyclists alike. Although much of the corridor has sidewalks and crosswalks, there have been several bicycle and pedestrian accidents along US 41.

## Transit Services

The US 41 Corridor is served by CCT Route 10, which provides local service from the Marietta Transfer Center (MTC) on South Marietta Parkway to Midtown Atlanta. A park-and-ride lot is also located at the MTC. CCT Route 45 provides service along US 41 from the MTC to Barrett Parkway and continues north to Kennesaw State University. In addition to these routes, there are several other CCT routes that cross US 41. Overall, the US 41 Corridor is one of the best corridors served by transit in Cobb County.

## Freight

US 41 is designated by GDOT as a truck route. Pursuant to GDOT data, truck traffic comprises approximately seventeen percent (17%) of the overall traffic along the US 41 Corridor.

### **LOS For Corridor – Cobb Parkway (US 41) Corridor**

<b>Segment of U.S. 41 Bartow County to Spring Road Corridor</b>	<b>Level of Service</b>
Bartow County Line to Cherokee Road	Mostly LOS A-C
Highway 92 to Acworth Due West	LOS D
Acworth Due West to Blue Springs Road	Mostly LOS A-C
Blue Springs Road to Mack Dobbs Road	LOS E
Mack Dobbs Road to Jiles Road	LOS E
Jiles Road to Cherokee Street	LOS F
Cherokee Street to Kennesaw Due West Road	LOS E
Kennesaw Due West Road to McCollum Parkway	LOS F
McCollum Parkway to Cole Street	LOS D
Cole Street to Fairground Street	Mostly LOS A-C
Fairground Street to North Marietta Parkway	LOS D
North Marietta Parkway to Terrell Mill Road	LOS C
Terrell Mill to ½ mile past Terrell Mill	LOS F
½ mile past Terrell Mill to Spring Road	LOS E
<b>Other Arterials</b>	<b>Level of Service</b>
Dallas-Acworth Highway	LOS A-C
Lake Acworth Drive	LOS F
Mars Hill Road	LOS D
Acworth Due West Road-Nance Road	LOS F
Jiles Road-Pine Mountain Road	LOS F
Kennesaw Due West Road	LOS D
Old Highway 41	LOS E
Chastain Road	LOS F
Barrett Parkway	LOS F
Bells Ferry Road	LOS D
Canton Road	LOS F
Allgood Road	LOS D
North Marietta Parkway	LOS D
South Marietta Parkway	LOS D
Roswell Street	LOS D
Delk Road	LOS D
Windy Hill Road	LOS F
Interstate Parkway North	LOS E
Circle 75 Parkway -Spring Street	LOS F

## Land Use and Community Resources

Given its length, there are a diverse range of land uses along the corridor. The corridor is comprised primarily of commercial development with a mixture of office, low to medium density residential development, and park/recreational/conservation land uses. There are many areas along the corridor that are transitioning from older retail strip commercial and industrial uses to redeveloped residential, office, and commercial uses.

## Access Management Issues

The US 41 Corridor has significant access management issues mainly as a result of excessive curb cuts and signage. Each shopping center generally has at least one ingress and egress point with little vehicle turn movement restrictions. In general, along much of US 41, the center lane has no barrier and is used for turning movements, stacking, and as a left turn lane. The merging of traffic in the middle lane in conjunction with traffic exiting left from retail establishments across two lanes of traffic causes a potential safety concern. However, within the Cumberland CID, planned landscaped, raised, medians will provide not only visual appeal, but also facilitate access controls.

## Corridor Needs Assessment

Output from the ARC 2030 Existing Plus Committed (E+C) travel demand model was utilized to identify future conditions that reflect the levels of congestion along the corridor. This model reflects conditions along the roadway network given projected growth patterns with capacities that would result from the completion of roadway projects that are currently programmed and/or in some phase of development. Model results are presented in the table below.

### **Existing and Future Travel Indicators – Cobb Parkway (US 41) Corridor**

<b>Traffic Indicator</b>	<b>2005</b>	<b>2030</b>	<b>Difference</b>
Total Vehicle Miles Traveled (VMT)	784,400	1,090,069	305,670
Travel Time Index (TTI)	1.36	1.93	0.57
PM Peak Hour Volume to Capacity Ratio (V/C)	0.70	0.92	0.22
Congested Speed	27.4	19.0	-8.4
Average Daily Traffic	44,620	62,021	17,401 (39%)
Average Daily Truck Traffic	3,200	5,228	2,028
Average Daily Truck Share (of VMT)	0.17	0.20	0.03

- Total Vehicle Miles Traveled (VMT) = The number of miles that residential vehicles are driven along the corridor.
- Travel Time Index (TTI) = The ratio of peak period travel time to free flow travel time. The TTI expresses the average amount of extra time it takes to travel in the peak relative to free-flow travel. A TTI of 1.3, for example, indicates a 20-minute free-flow trip will take 26 minutes during the peak travel time periods, and therefore a 30 percent travel time penalty is incurred.
- PM Peak Volume to Capacity Ratio (V/C) = The actual volumes along the roadway compared to its design capacity. For example a roadway designed to carry 10,000 trips during the peak hour with a PM peak hour of 12,000 trips would have a V/C ratio of 1.2, which would indicate that it is carrying 20 percent more traffic than its design capacity.
- Congested Speed = A congested speed (mph) accounting for the effects of delay (min/mi) on the free-flow speed (mph). A travel speed lower than 60% of the posted speed limit is an indicator of delay, which can result in congestion.

As reflected in table previously, roadway conditions are projected to worsen by 2030. Travel along the corridor is projected to increase considerably, resulting in increased delays and lower overall travel speeds. As shown previously, most of the US 41 Corridor is currently operating at or above capacity. Truck traffic is also projected to increase, which also has the potential to create added conflicts along the roadway in light of the difficult access management issues that currently exist. As such, the planned capacity and operational improvements along the corridor are needed. Notwithstanding, additional capacity and/or operational improvements may be necessary in order to maintain an acceptable LOS along the corridor through 2030. Said improvements also need to address the difficult access management issues along the corridor and, thus, increase safety along the roadway.

The level of traffic volumes and operational characteristics along the US 41 Corridor present a serious challenge to bicycle and pedestrian travel. However, better connectivity for both bicycle and pedestrian facilities is needed to provide access to transit services and the high level of retail development along the corridor. The high number of bicycle and pedestrian accidents demonstrates a significant demand for these modes of travel along US 41 despite its poor suitability for these modes of travel.

### Need for Additional Improvements

As shown in the table above, there are numerous improvements planned along the US 41 Corridor. Most improvements are capacity and/or operational improvements on roadways that feed into the US 41 Corridor, along with several operational improvements. The widening projects along US 41 south of Marietta should provide adequate capacity along that portion of the corridor. Given the level of congestion projected for the roadway in 2030, the following additional improvements (in conjunction with the capacity improvements to intersecting roadways) are recommended for consideration:

- Widening of US 41 from Canton Road to Cedarcrest Road from four to six lanes with access management controls and design elements to accommodate truck traffic and promote bicycle and pedestrian safety along the roadway;
- Expansion of transit service west of Barrett Parkway;
- Intersection improvements at Nance Road/Acworth Due West Road;
- Intersection improvements at Mack Dobbs Road; and
- Intersection improvements at SR 176 (Mars Hill Road).

## TIP AND SPLOST PROJECTS IN THE VICINITY OF THE CUMBERLAND CID

Roadway	Boundaries	Committed Projects		Potential Future Projects
		SPLOST	TIP	RTP
Akers Mill Rd	Cumberland Blvd to 250 ft. west of Cobb Galleria Pkwy		Bike/ped improvements	
Cobb Galleria Pkwy	Akers Mill Rd to I-75 north overpass		Bike/ped improvements	
Cobb Pkwy	Fulton County limit (Mount Paran Rd) to Paces Mill Rd	Congestion relief (widen bridge to 6 lane divided)		
	Paces Mill Rd to Akers Mill Rd	Congestion relief (widen to 6/8-lane divided)		
	Paces Mill Rd to Akers Mill Rd		Roadway capacity	
	Akers Mill Rd to Windy Ridge Pkwy			
	Windy Ridge Pkwy to Windy Hill Rd			Roadway capacity
	Circle 75 Pkwy to Windy Ridge Pkwy		Pedestrian improvements	
Cumberland Blvd	At Cumberland Transfer Center	S&O (median safety improvements)		
	US 41 (N) to US 41 (S)			
Cumberland/Galleria Airport Connector	HJAIA to MTC via I-285 and Cobb Pkwy			
Cumberland/Galleria Circulator	Cumberland/Galleria area			
Galleria Park and Ride Lot	Along Cobb Galleria Pkwy		CO-AR-303	
Leland Dr	Northern terminus to Terrell Mill Rd			Extension
Paces Ferry Rd	At Chattahoochee River	Bridge engineering		
Powers Ferry Rd	Delk Rd to Terrell Mill Rd			Widen
Rottenwood Creek Trail	Paces Mill NPS to Terrell Mill Rd			
Spring Rd	Campbell Rd to Cumberland Blvd	Bicycle facilities		
Spring Rd Multiuse Trail Cumberland Connector	Campbell Rd/Carolyn Dr - to I-285		Pedestrian overpass (CO-AR-BP221)	
Terrell Mill Rd	Delk Rd to Old Canton Rd	S&O (intersection improvements)		
Windy Hill Rd	At I-75	Congestion relief (interchange improvements)		
	At Powers Ferry Rd	Intersection Improvements		

## **SIGNAGE AND WAYFINDING STUDY** **(December 2004)**

The creation of a signage program is complementary to the branding component. The report recommends the following hierarchy of signage: GATEWAY—DIRECTIONAL/WAYFINDING—PARKING ID—PEDESTRIAN/KIOSK. An image or brand is accomplished through colors and logos, type of materials used, and forms on signs. Not including the cost of design and retrofitting existing signs, the program is estimated at \$ 1.52 million.

The following issues were presented in the study:

- Main thoroughfares through the project are high traffic volume.
- There is limited R/W on many roads (Cobb Parkway and Cumberland Blvd.).
- Design schematics for streetscapes need to consider sight lines for signage.
- Current sign ordinances apply to only business/commercial, not public signage.
- Design speeds along Cobb Pkwy. Lead to need for minimal listings and quick processing of sign information.
- No consistency on which to base sign program. No existing points of entry to and exit from the site.
- Wide intersections need large scale solutions.
- Need consistently placed landmarks to identify entry and usage points. Current markers and identifiers are mismatched.
- Lighting along primary pathways is inconsistent. Need reflectivity of type on directional/wayfinding signage.

“The final result should be a program of directional signage with the flexibility to be placed on posts, panels, or existing infrastructure, yet to be recognizable and consistent.”

## **COBB GALLERIA PARKWAY STREETScape PROJECT**

### **Need and Purpose:**

The proposed Livable Centers Initiative (LCI) project is part of the Cumberland Community Improvement District's (CCID) master plan "Blueprint Cumberland." Cumberland/Galleria is home to 30,000 residents and 65,000 workers within 10 square miles and contains Cumberland Mall. The close proximity of homes, offices, and retail centers creates heavy pedestrian and vehicular traffic. The proposed multi-use path will improve safety as well as provide a "front door" to the Performing Arts Centre under construction on Cobb Galleria Parkway. Landscaping will be added on both sides of the street creating a pedestrian-oriented street that will provide an appropriate venue for public art. The project will tie into existing sidewalks on both Akers Mill Road and Cumberland Boulevard. The proposed project is in the current State Transportation Improvement Program (STIP). The project ties to another streetscape project along Akers Mill Road, STP-0006-00(289).

### **Description of the proposed project:**

The Cobb Galleria Parkway Streetscape project consists of constructing a 10-foot multi-use path along the east side and a 5-foot sidewalk on the west side of Cobb Galleria Parkway. The project will include landscaping and lighting on both sides of the roadway and enhanced crosswalks (stamped asphalt/concrete, etc.). The project is approximately 3,000 feet in length.

## **AKERS MILL ROAD STREETScape PROJECT**

Akers Mill Road is located in Cobb County, Georgia. The project corridor begins at Cumberland Blvd SE and ends at Camp Parkway. The project corridor crosses both US 41 and 1-75. The project will enhance the pedestrian streetscape with a sidewalk and the addition of a multi-use path. The project will provide a 5'-8' wide sidewalk on the north side of Akers Mill and a 10'-12' wide multi-use trail on the south side of Akers Mill. The project will also include sidewalks, site furnishings, and landscaping in a small urban park at the south east corner of the intersection of Akers Mill and US 41.



## **I-285 INTERCHANGE BEAUTIFICATION PROJECT**

The proposed project would provide landscaping and other beautification improvements at the existing interchange area of I-285 and Cobb Parkway/SR 19/SR 41. The I-285 Interchange Beautification project would be constructed completely within existing right-of-way. The total project length would be approximately 0.65 mile.

The interchange area would be enhanced with landscaping. The plant pallet would be diverse, but practical from a maintenance perspective, using large masses of material for ease of viewing at high rates of speed. Since the interchange acts as a gateway to the Cumberland/Galleria area, landscaping would reflect the need to provide wayfinding to the area for businesses and consumers. The use of trees of stature at all four corners of the I-285 interchange, which can be viewed from the interstate, will provide that gateway appeal. Non-traditional materials such as structural soils and cultivar species will be used in the landscaping to enhance the viability and beauty of the plants. Treatment of the zone beneath the I-285 bridge will be given special attention and may include such items as vandal proof accent lighting, staining of concrete and trash receptacles. All improvements would meet minimum clear-zone requirements, based on a design speed of 45 miles per hour for the on- and off-ramps of I-285 at Cobb Parkway. Per the local maintenance agreement, the proposed interchange would be maintained by Cobb County and the Cumberland Community Improvement District (CCID). Irrigation would be provided and an indemnity agreement between GDOT and Cobb County would be obtained. The proposed interchange would be maintenance-free for GDOT. Total estimated cost of \$555,000.

## **COBB PARKWAY PEDESTRIAN IMPROVEMENTS**

The proposed project would upgrade the existing pedestrian facilities for the traveling public, improve pedestrian safety, and provide traffic calming techniques and aesthetics in the project area. The Cobb Parkway Pedestrian Improvements project would create a safe, user-friendly public space for pedestrians along Cobb Parkway/SR 19/US 41 without impacting vehicular traffic.

Cobb County is located within the 13-county Atlanta non-attainment area for air quality. The proposed project would support efforts to reduce dependence upon automobile-oriented development by investing in a developed area, to increase pedestrian infrastructure, which is connected to neighborhoods, schools, businesses and parks via a local street network. The proposed project would support Atlanta Regional Commission (ARC), U.S. EPA and regional efforts to “foster greater livability in activity and employment centers in our region.” The project would provide funding to enhance livability and mobility for residents, and support the fundamental concepts of:

- Connecting homes, shops and offices;
- Enhance streetscaping and sidewalks; and,
- Emphasizing the pedestrian.

The proposed project is a Livable Centers Initiative (LCI) and is part of the Cumberland Community Improvement District (CCID) master plan “Blueprint Cumberland.” Cumberland/Galleria is home to approximately 30,000 residents and 65,000 workers within 10 square miles and contains Cumberland Mall and the Galleria Specialty Mall. The close proximity of homes, offices and retail centers creates heavy pedestrian and vehicular traffic.

### Logical Termini:

The project limits constitute a heavily traveled corridor connecting hotels and other commercial properties on the west side of I-285 to the Galleria Specialty Mall and Cumberland Mall located on the east side of I-285. The proposed project would provide an alternative to vehicular travel along the corridor. The proposed sidewalks would connect with the existing sidewalk facilities located on both sides of Cobb Parkway/SR 19/US 41 and to multi-use trails and green-space located on Akers Mill Road.

### Project Description:

The proposed project would enhance existing pedestrian facilities by providing a streetscape and upgrading the existing sidewalks and crosswalks located on both sides of Cobb Parkway/SR 19/US 41 from Circle 75 Parkway to Akers Mill Road. Median improvements and signing improvements are also proposed. All construction would conform to the Americans with Disabilities Act (ADA) regulations. The total project length would be approximately 0.85 mile.

The proposed project would widen the existing sidewalks to provide 12-foot pedestrian streetscapes, including sidewalks, where possible. The streetscape would extend from the existing curb so that the existing curb and roadway would not require modification. Minimum slopes of 4:1 would be provided. Non-traditional materials such as structural soils and cultivar species will be used in the landscaping to enhance the viability and beauty of the plants. Irrigation would be provided and an indemnity agreement between GDOT and Cobb DOT would be obtained.

Crosswalks would be constructed using the GDOT ladder crosswalk design standard that combines the transverse striping of a basic sidewalk with the higher contrast “piano key” markings parallel to traffic flow placed over stamped asphalt accented for design consistency within the Cumberland CID. Crosswalks at other side street locations would be upgraded to meet current ADA requirements. Additional pedestrian lights with breakaway design, asphalt stamping and bollards would be utilized at the crosswalks.

Median improvements are also proposed to calm traffic, improve pedestrian safety, and provide aesthetics to the project area. The existing paved medians would be replaced with landscaped islands. The proposed project would provide improved pedestrian signage with breakaway posts along the streetscape. Signage to public facilities in the area would also be provided.

Existing right-of-way is variable between 170 and 250 feet. The proposed project would be constructed primarily within existing right-of-way. Minor amounts of right-of-way would be required, and temporary construction easements are anticipated. The estimated cost is \$1.837 million.

## **SE QUADRANT STUDY**

As a proactive measure, the Cumberland CID initiated a transportation master plan to provide an efficient, multi-modal transportation system for the SE Quadrant of the CCID that will address area needs now and the immediate future.

The study area for the Southeast Quadrant Transportation Master Plan includes the area surrounded by the Cumberland Boulevard “loop,” Circle 75 Parkway, Windy Ridge Parkway, and Interstate North Parkway. The study area transportation network consists of roadways, transit routes, sidewalks, and multiuse trails. The study area transportation infrastructure is primarily automobile-oriented. However, the study area is well-served by bus transit, and a major transfer center is located on the southern edge along the west side of Cumberland Boulevard.

**Table 3b: Planned Improvements – Transit**

<b>Map No.</b>	<b>Facility</b>	<b>Project</b>	<b>From/At</b>	<b>To</b>	<b>Completion Date</b>
9	Galleria Park-and-Ride Lot	Park-and-ride lot	Performing Arts Centre	N/A	2006
10	I-75 North	BRT	Akers Mill Road	Town Center	2011
11	I-285 North	BRT	Cumberland	Perimeter	2012
12	U.S. 41	Arterial BRT	Marietta	Cumberland-Galleria Transfer Center	2020
13	U.S. 41	Arterial BRT	Cumberland/Galleria Transfer Center	MARTA Arts Center Station	2016

### Physical Characteristics

- Average sidewalk segment length (1,180 feet) is considerably longer than average downtown blocks (250 feet)
- Seventy-nine percent of the 106 study segments have sidewalks
- More than 60% of the segments rated good to optimal in overall condition and cleanliness
- Seventy-one percent have continuity and most were obstacle free

### Perceived Safety

- Ninety-three percent do not have a buffer between the sidewalk and the roadway
- Fifty-two percent do not have lighting
- Seventy-three percent do not have visibility to surrounding developments
- Thirty-eight percent have crosswalks for all pedestrian movements

### Sidewalk Appeal

- Seventy-six percent accessed no destinations/attractions
- Thirty-four percent are adjacent to surface parking
- Ninety-nine percent do not have street furniture
- Thirty-nine percent do not have landscaping and 29% had partial segments landscaped

Table 3c shows planned bicycle and pedestrian projects.

**Table 3c: Planned Improvements – Bicycle/Pedestrian**

Map No.	Facility	Location	Status
14	Rottenwood Creek Trail <u>aka</u> (Bob Callan Trail)	Interstate North Parkway to the Chattahoochee River	Programmed
15	Cobb Parkway Trail	Spring Road to Akers Mill Road	Programmed
16	Spring Road Trail	Adjacent to Spring Road	Programmed
17	Rottenwood Creek Trail	Windy Ridge Parkway to Interstate North Parkway	Planned
18	Cobb Parkway Trail	Akers Mill Road to Cumberland Boulevard	Programmed
19	Silver Comet Cumberland Connector	U.S. 41 (Cobb Parkway) to Cobb- Galleria Parkway	Planned

Congestion, measured by average vehicular delay, is 37% greater in the PM peak period than in the AM peak period. The majority of intersections (94% - AM and 91% - PM peak period) are currently operating at an acceptable LOS. During the AM peak period, Spring Road at U.S. 41 (Cobb Parkway) and the I-285 eastbound ramp at U.S. 41 (Cobb Parkway) are the only intersections operating at an unacceptable LOS. Both intersections are operating at LOS E.

During the AM peak hour, the eastbound right-turn movement onto Cobb Parkway at the intersection of Spring Road accounts for 40% of the southbound approach volume on Cobb Parkway. During the PM peak period, Spring Road at Cumberland Boulevard, Cumberland Parkway at Cumberland Boulevard, and the I-285 westbound ramp at U.S. 41 (Cobb Parkway) are the only intersections operating at an unacceptable LOS. All three intersections are operating at LOS E. These three intersections account for about 28% of the delay in the SE Quadrant. These intersections are not the same as those that account for the majority of AM peak-period delay. Therefore, transportation investments made to address AM peak-period delay will have minimal benefits on delay in the PM peak period.

During the PM peak hour, the westbound right-turn movement onto Cobb Parkway at the intersection of the I-285 westbound ramp at Cobb Parkway accounts for 41% of the northbound approach volume on Cobb Parkway. Rear-end collisions are the predominant type in the study area, accounting for 52% of crashes. The four intersections having the highest collision rates are also among the top five intersections by volume and delay for both the AM and PM peak periods.

Traffic conditions were forecast for the year 2030 based on the Atlanta Regional Commission's (ARC's) 2005 and 2030 travel demand models and land use data including area size, employment, households, and population.

### **Findings and Observations**

- The network traffic volume for the year 2030 is approximately 35% greater than the existing traffic volumes.
- Traffic volumes during the PM peak hours are 22% greater than volumes during the AM peak hours.
- The future year network delay (measured in terms of average vehicular delay) during the AM peak hours is 84% greater than the existing condition.
- The future year network delay (measured in terms of average vehicular delay) during the PM peak hour is 95% greater than the existing condition.
- Congestion (measured in terms of average vehicular delay) is 41% greater in the PM peak hours than in the AM peak hours.
- Three (3) of 32 signalized intersections operate at an unacceptable Level of Service (LOS) of F, during the AM peak hours.

Spring Road / Circle 75 Parkway at Cobb Parkway  
I-285 Eastbound Ramp at Cobb Parkway  
I-75 Southbound Ramp at Cumberland Boulevard

- Six (6) of 32 signalized intersections operate at an unacceptable Level of Service (LOS) of E and below, during the PM peak hours.

Cumberland Boulevard / Windy Ridge Road at Cobb Parkway

Spring Road at Cobb Parkway

I-285 Westbound Ramps at Cobb Parkway

Spring Road at Cumberland Boulevard

Cumberland Parkway at Cumberland Boulevard

Akers Mill Road / Stillhouse Road at Cumberland Boulevard

### **Plan Implementation-- High-Priority Projects**

<b>Intersections</b>				
1	Re-stripe the I-285 off ramp from 2 left turn lanes and 3 right turn lanes to 3 left turn lanes and 2 right turn lanes	US 41 (Cobb Parkway)	I-285 eastbound off ramp	N/A
2	Add concrete barrier to separate left turn lanes and through lanes between intersections	US 41 (Cobb Parkway)	I-285 westbound off ramp	Spring Road
<b>Sidewalks</b>				
3	Cumberland Boulevard West	Cumberland Boulevard	Spring Road	Akers Mill Road
4	Cumberland Boulevard Southeast	Cumberland Boulevard	Akers Mill Road	US 41 (Cobb Parkway)
5	Cumberland Boulevard South	Cumberland Boulevard	US 41 (Cobb Parkway)	Overton Park South
<b>Streetscape</b>				
6	Spring Road	Spring Road	Cumberland Boulevard	US 41 (Cobb Parkway)
7	Akers Mill Road West	Akers Mill Road	Cumberland Boulevard	US 41 (Cobb Parkway)

**Note: These projects have been included in the “Proposed Projects and Initiatives” list provided later in this report.**

## CUMBERLAND GALLERIA CIRCULATOR STUDY

The Cumberland/Galleria Circulator Study, completed by URS for the Cumberland CID, analyzes the need for a circulator system in the Cumberland/Galleria area. The needs evaluation was formulated by comparing six circulator systems generally similar to the Cumberland/Galleria area in terms of geographic area, population, density, urban form, economic base, housing, attractions, and employment patterns. These areas deemed to be evaluated and compared by URS and CCID are:

- Buckhead Community Improvement District (Atlanta, GA)
- Contra Costa Centre Association (Walnut Creek, CA)
- Emeryville Transportation Management Association (Emeryville, CA)
- Chatham Area Transit Authority (Savannah, GA)
- City of Scottsdale, AZ
- The Wave (Wilmington, NC)

The study found a strong need for a circulatory system in the Cumberland/Galleria area and that the system would serve a number of desired purposes such as:

- Tie in Cumberland/Galleria community
- Provide a regionally unique mobility option and enhancement of the visual environment
- Supply direct and reliable service to the Cumberland/Galleria area
- Encourage greater use of public transit
- Minimize congestion

While examining the circulatory systems of the peer areas, special consideration was given to important common elements which include:

- Links large numbers of destinations and activities
- Regular and frequent service
- Linear route design within the activity areas
- Simplicity in understanding, using, and accessing the service
- Connectivity with other area transit services.

After the study found that there is a substantial need for a circulator system in the Cumberland/Galleria area, it formulated a system to implement. During the alignment study for the Cumberland/Galleria circulator system's route the study found five main attractions which consisted of the Cumberland Mall, Galleria Mall, Cumberland/Galleria Conference Center, Cobb Community Transit (CCT) Cumberland Transfer Center, and Office Parks. These five attractions were marked by the study to be must stop locations. The study then created a map displaying them on the purposed route. Along with these five attractions the map also consists of future development projects and areas for future growth.

Along with the route map, the study developed service parameters that are as follows:

- Service provided Monday-Saturday
- Service hours approximately 9:30a.m.-5:30p.m.
- Service frequency options for 10 and 15 min comparison
- Service will be free
- 

The study also purposed that the route tie in with CCT routes, pedestrian / bicycle paths, and the future GRTA BRT system.

During the peer area review the study compared the five area's service efficiency, cost effectiveness, and service effectiveness. Also compared between the five areas were their operating characteristics. These characteristics are: number of peak vehicles; annual passenger trips; annual operating cost; annual vehicle revenue hours; and annual vehicle revenue miles. From the information collected on these characteristics the study calculated five performance measures:

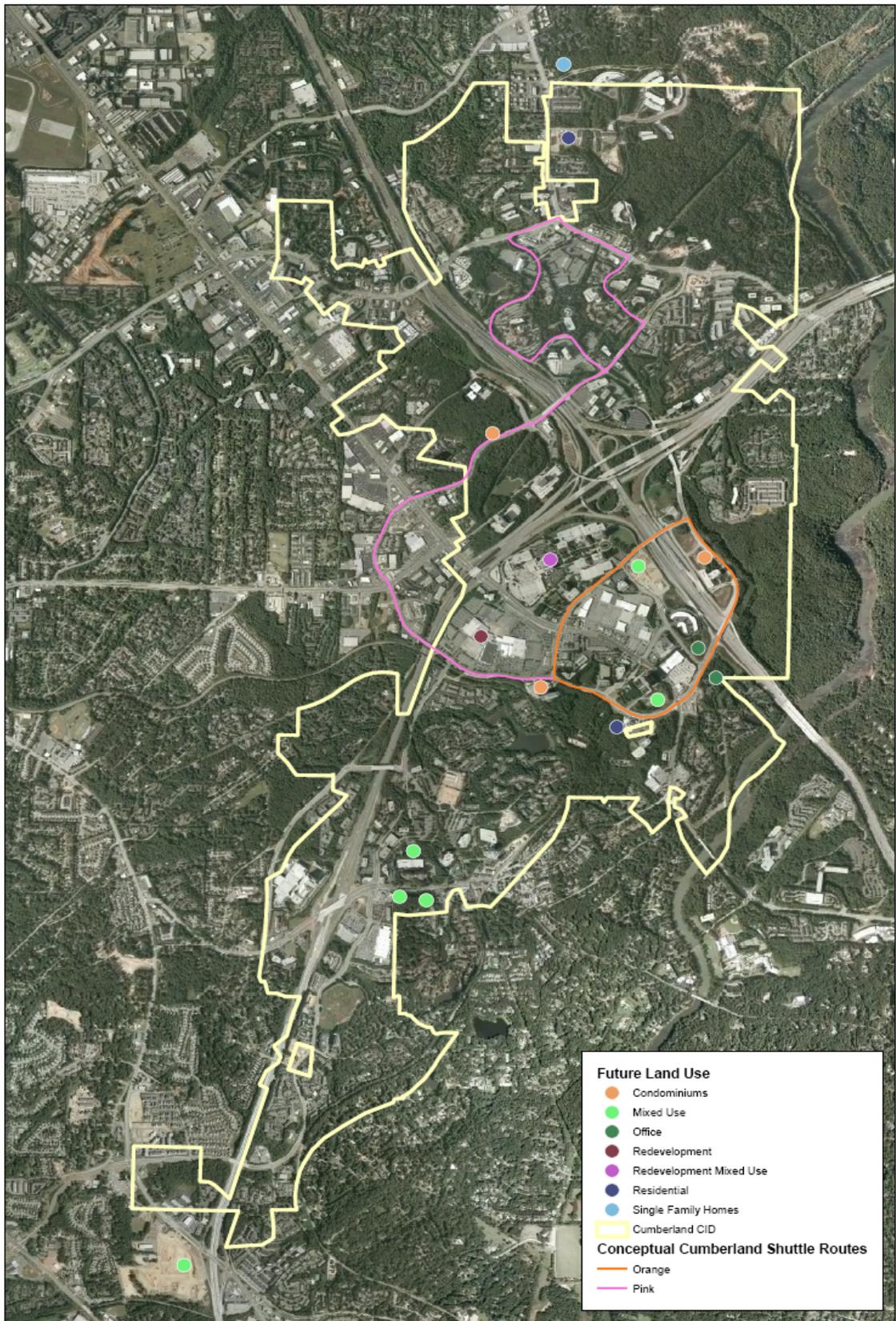
- Operating expenses per vehicle revenue mile;
- Operating expenses per vehicle revenue hour;
- Operating expenses per passenger trip;
- Passenger trips per vehicle revenue mile; and,
- Passenger trips per vehicle revenue hour.

Looking at the information, the study found that the potential Cumberland/Galleria circulator system would perform better than average on the first measure until the highest measure, Emeryville, is removed. Cumberland/Galleria is also higher than average on the second measure, which means it would perform worse than the peer average. On the third, fourth, and fifth measures the Cumberland/Galleria falls slightly lower than the peer average.

After analyzing the data and measures of the peer areas the URS / CCID study proposed a circulator service with details describing the route alignment, level of service options, and potential implementation phasing.

Phase 1 would be comprised of two routes both originating at the Cumberland/Galleria Center. One would travel around the central mall and the other would operate to the office park. The route characteristics are that of a two-directional linear service in which stops are made at key attractions and that the round trip length is 8.6 miles. Along with the route characteristics an operating plan for 10 and 15 minute frequencies was developed. The map of the routes is on the next page.

Estimated ridership was also calculated and it was found that the main circulator ridership would come from visitors, Cumberland/Galleria Square shoppers, diners, business trips, employment trips, miscellaneous community trips, and CCT riders. When looking at a moderate ridership scenario year 1 would have 36,100 riders (50% of Estimate), year 2 would have 54,150 riders (75% of Estimate), and year 3 would have 72,200 (100% of Estimate). Conservative and aggressive ridership scenarios were also created and their numbers fluctuated accordingly.



0 0.25 0.5 1 Miles

## **HOV/BRT STUDY**

The proposed project currently under consideration provides for the extension of high occupancy vehicle (HOV) lanes on I-75 north from Akers Mill Road to north of Hickory Grove Road, construction of new HOV ramps and interchanges for access to the system, and implementation of express bus or bus rapid transit (BRT) services supported by transit stations, park-and-ride lots, and various bus priority improvements. Additionally, HOV lanes are also proposed on I-575 from the I-75/I-575 Interchange to Sixes Road in Cherokee County. Truck Only Lanes (TOL) are also proposed for consideration on I-75. The truck only lanes would be constructed within the same limits on I-75 as the HOV lanes and would be barrier separated from the general purpose and HOV lanes. GDOT has assumed the role of lead state agency in the preparation of the environmental studies, in cooperation with GRTA and the corresponding federal agencies, the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA).

The station area planning process is designed to involve the community in the development of land use concepts and pedestrian and traffic circulation plans for proposed BRT stations. The overall objective of this process is to carefully fit the BRT stations into the context of the areas in which they will reside; assuring successful operations while supporting the aspirations and needs of the community. The resulting land use plans developed with community will be recommended to Cobb County and the City of Marietta for implementation. Participants also have the opportunity to comment on and influence design concepts for the station facilities.

In addition, an advisory steering committee has been organized by GRTA. The steering committee is comprised of staff members from Cobb County, City of Marietta, and area Community Improvement Districts. The committee meets on a regular basis to oversee and coordinate the station area planning efforts.

The stations at the following locations are the subject of the station area planning program:

- Town Center Station: Big Shanty Road Extension at I-75 near Town Center
- Marietta Station: Between Roswell Road and Gresham Road on I-75
- Franklin Station: Midway between Delk Road & South Marietta Parkway on I-75
- Terrell Mill Station: Terrell Mill Road at I-75
- Cumberland Station: Akers Mill Road at I-75

The BRT station locations at Akers Mill Road, Delk/Franklin Road and Big Shanty Road have completed planning efforts as part of the Atlanta Regional Commission's Livable Centers Initiative program. The Northwest Corridor Project Team is coordinating the results of those plans with the station area planning process.

Phase I of the station area planning began in March and continued through the summer of 2005. Seven meetings were held with over 350 attendees from area businesses, neighborhoods and organizations. Phase II began in January of 2006. This next phase will focus on creating design guidelines for development around the stations as well as refining some of the pedestrian access and circulation issues identified in the last phase.



**Source:** Georgia Regional Transportation Authority

## **I-285 STRATEGIC IMPLEMENTATION PLAN**

The Georgia Department of Transportation is developing a Strategic Implementation Plan for I-285, metropolitan Atlanta's heavily traveled perimeter highway. I-285 has been widened twice since its completion in 1969, initially to 8 lanes in 1989 and later in 1996 to 10 lanes along its northern quadrant, because of intense development along the corridor. The Atlanta Regional Commission's (ARC) Mobility 2030 Regional Transportation Plan (RTP) has identified several new capital improvements for I-285, including the addition of HOV lanes and the reconstruction of several existing interchanges. GDOT will meet its mission to maintain the safe and efficient operation of this corridor. As part of the analysis, simulation tools are being developed and utilized to interface the urban transportation model with detailed characteristics of the operation of I-285 and its connecting freeways and arterial roadways. The result of this effort will be a Strategic Implementation Plan for I-285 that both refines the recommendations from ARC's RTP and identifies a variety of projects to ensure I-285's safe and efficient operation in the future.

The Department and consultant team completed the selection of the software platforms to be used for the transportation simulation model. The study team is currently in the process of final calibration of the model designed specifically for this study. Several initial concepts have recently been developed for evaluation. The initial concepts are concentrated in the northern quadrant of I-285 from I-75 (north) to the Stone Mountain Freeway. The process of assessing the concepts will involve applying various design options to each simulation looking for viable improvement alternatives. The project's Stakeholder Advisory Group held its first meeting in October, 2005 to provide input and policy guidance to facilitate the analysis effort. A second stakeholder meeting was held in April, 2006. Results from this project will serve as valuable input into the GDOT's *North Atlanta Interstate Corridor Transportation System Improvement Project*.

## **CUMBERLAND LCI HOUSING STUDY**

Several key housing issues were identified as being direct or indirect factors relating to the future economic quality and strength of Cumberland/Galleria and its massive office market, including:

- An increasing imbalance between jobs and housing in the area. As nearby housing prices escalate, especially for-sale housing, this imbalance causes lower and middle income households to drive to jobs in the area from increasingly greater distances;
- The potential for the jobs/housing imbalance to cause further degeneration in local and regional transportation mobility as the area develops the remaining 50% of available developable (or redevelopable) property;
- The growing potential for the jobs/housing imbalance to become an impediment to future economic development in the area;
- A lack of homeownership, resulting in a high proportion of renters in the area;
- The potential deterioration and decline of portions of the existing housing stock, particularly the large aging rental stock on the fringes of the area; and,
- A lack of housing in the Core Area of the Cumberland CID, inhibiting its vitality and potential to emerge as a “24-hour” commercial submarket area, the type of area where the greatest commercial investments will be made.

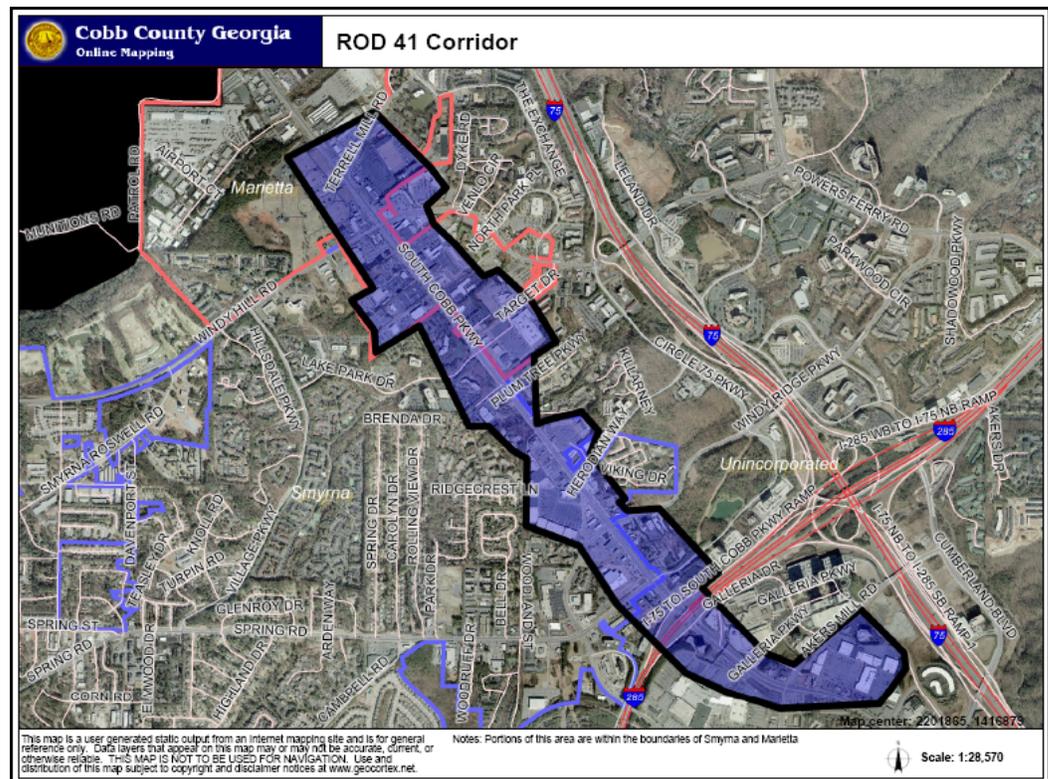
Current housing policies have lead to:

- Artificially “propping up” older apartment properties by constricting supply while demand for rental product in the area has increased;
- Limiting development of new rental units which, in areas such as Midtown and Buckhead, have accounted for a significant share of units converted to condominium ownership; and,
- Limited financing options for direct condo construction, by limiting opportunities for developers to finance new residential construction as apartments (which has less stringent financing requirements relative to condo development).

The Housing study further stresses that key changes are necessary to Cobb's Comprehensive Plan, both inside the core of Cumberland and in the adjacent areas. "This includes not only amending the Comprehensive Plan, but providing zoning changes as well. Within the core of Cumberland, [we should] amend office zoning to better qualify mixed-use development as an equally opportune development opportunity to single-use office and amend retail/ service areas to allow residential development. As an alternate, create a new zoning category more permissive of intense mixed-use development in the Core Area. Outside of the core, the major focus shifts to allowing redevelopment of rental apartments to a mix of for-sale and rental housing as demonstrated earlier in this report." The housing report recommends that the county and the Cumberland CID should provide "for more dense areas within the redeveloping projects, as well as requiring a development pattern more conducive to walking and to transit is encouraged. This will require changes in the Comprehensive Plan and its recommended development policies. It will also necessitate zoning policy changes. Whether accomplished through new zoning categories or through an overlay zone, creating more flexibility to allow development of workforce for-sale housing is recommended. In addition, design guidelines intended to set minimum standards for pedestrian-based site planning and material quality should be considered for the core and surrounding areas. Design guidelines can be voluntary, can be adopted by the CCID as a template for desired development using public space improvements as an incentive, or can be enacted and enforced by the County."

## COBB COUNTY'S REDEVELOPMENT OVERLAY DISTRICT

“The ROD is established to provide locations for redevelopment of commercial, office and residential uses which are pedestrian oriented and developed at a community or regional activity center scale and intensity, as identified for each specific site or corridor via the Cobb County Comprehensive Plan, as may be amended from time to time. This is intended to encourage compatible mixed-uses within the boundaries of these properties. The district may be overlaid upon the LRO, LRC, NRC, O&I, CRC, RMR, OMR, RHR, OHR, NS, PSC, TS and GC zoning districts within these redevelopment corridors and specific redevelopment sites. The district may also be overlaid upon the RM-12 and RM-16 districts that are adjacent to commercially zoned properties within these redevelopment corridors and specific redevelopment sites.” (Sec. 134-221.2, *Redevelopment Overlay District (ROD), Cobb County Code*). Developments within the ROD must comply with the urban design standards developed as part of Blueprint Cumberland I.



**Source:** Cobb County Community Development, Planning Division

## **COBB COUNTY COMPREHENSIVE PLAN - 2030**

Blueprint Cumberland II's vision should be consistent with the 2030 Vision for Cobb County as espoused in the County's Comprehensive Plan document.

### Vision Statement

*"In the year 2030, Cobb is recognized as Georgia's most complete community, a place that combines the best of urban, suburban, and rural life to appeal to a broad spectrum of people.*

*Cobb is proud of being a regional leader and supportive partner in making the Atlanta Region a great place to live, work, and play.*

*Residents in Cobb County live comfortably in a safe community that has quality jobs, housing choices, cultural activities, recreational opportunities, and state of the art educational/health services.*

*Cobb takes pride in actively preserving its natural resources. We have achieved adequate water supply to support the need of our citizens and businesses. In addition, we have taken steps to ensure water and air quality for the community.*

*Out of respect for its natural resources, Cobb promotes and preserves parks, trails, greenspace, tree canopy, and sensitive habitat. Due to our unique history, we continue to protect and maintain historic features.*

*Cobb is making progress towards resolving issues concerning traffic and mobility. Congestion times have decreased and there are more mobility options available to the community.*

*Cobb is known as a community with low taxes; effective, efficient, and trustworthy government; balanced attitude that supports both businesses and neighborhoods; an outstanding record of public safety; strong community pride; generosity of its citizens to those in need; and quality choices for its people.*

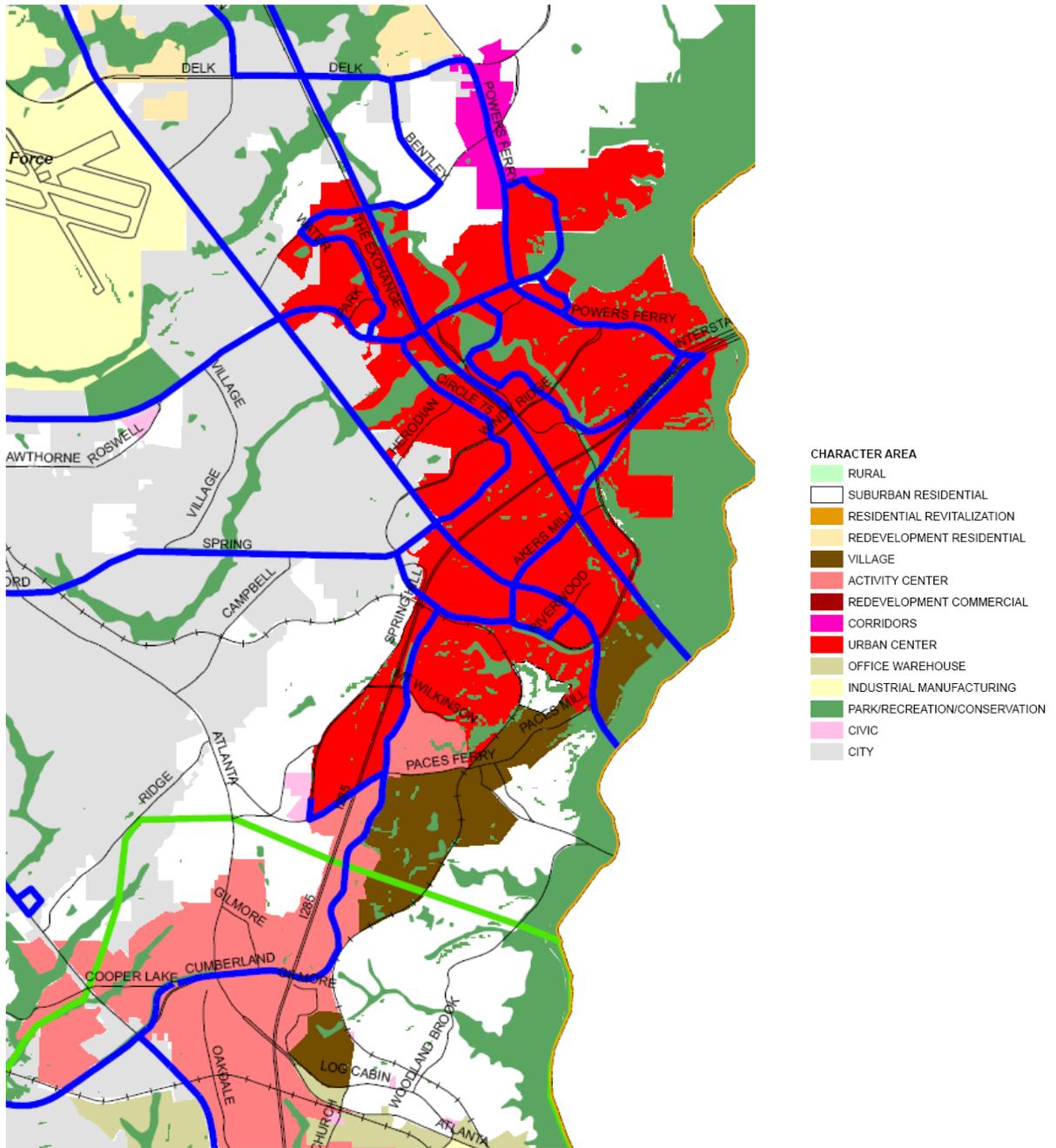
*Cobb supports its citizens and benefits from their service, leadership, and personal accountability. People of all backgrounds, races, cultures, ages, religions, national origins, and genders feel comfortable living and working in Cobb County.*

*All of these outcomes can be attributed to Cobb's way of reaching out to its citizens and businesses, involving them in important decisions and working cooperatively to determine the best overall course of action."*

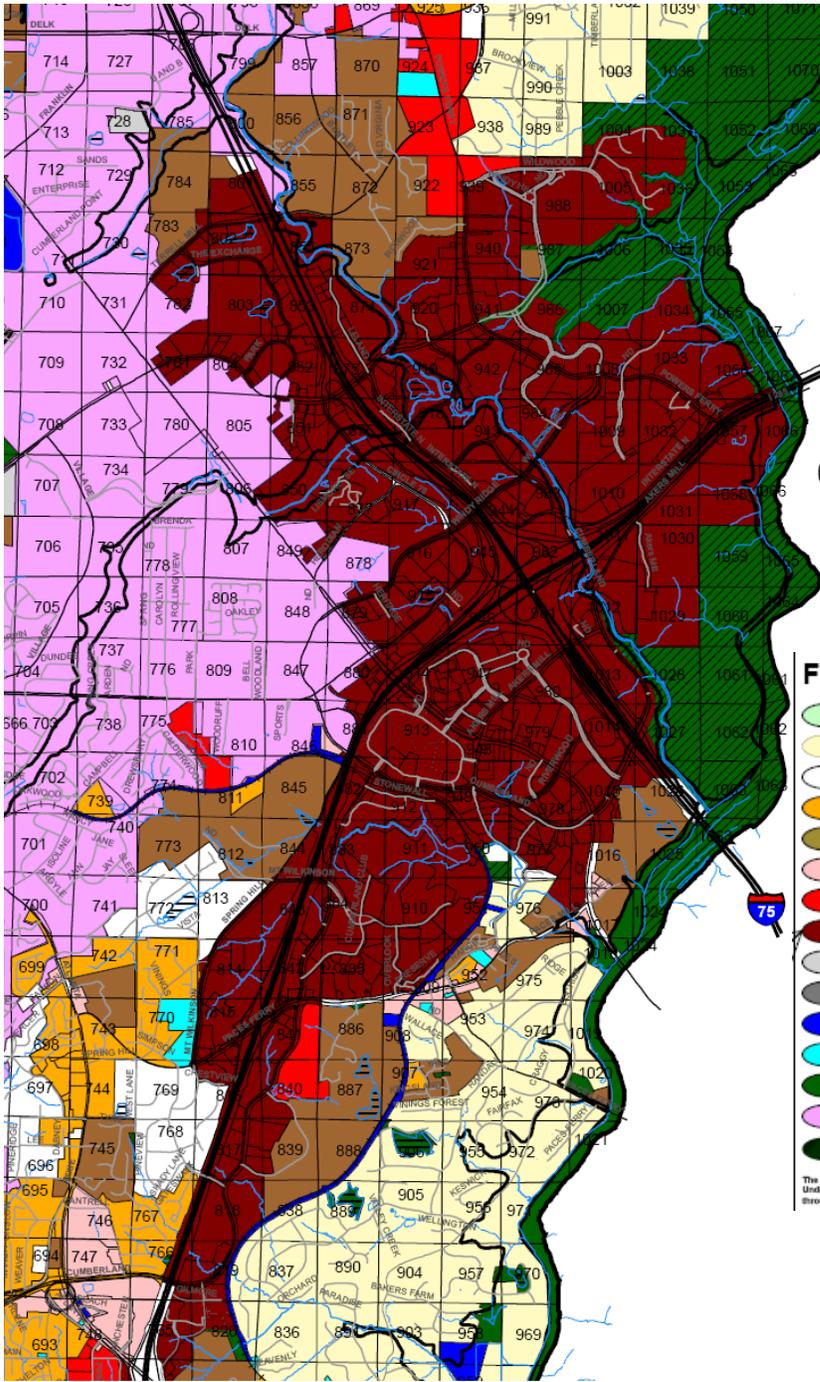
### **Character Areas**

According to the proposed Comprehensive Plan 2030 for Cobb County, character areas are defined as "unique areas of the county that contain characteristics that separate them from the surrounding areas. They exhibit development patterns, urban design, special architectural elements, history, stability, blight, or some other factor that will assist others in understanding more about the area." The character area map will not become Cobb County's policy tool, since the County will continue to use its Land Use Map for such decisions. As such, the character area map is designed "to help create a vision for the future of the county and will be used to develop the goals and policies that are included in its 2030 planning document. The Cumberland/Galleria area is described in the Cobb County Comprehensive Plan as mostly "Urban Center."

# CHARACTER AREAS—COBB COUNTY COMPREHENSIVE PLAN



**Source:** Cobb County Community Development, Planning Division



# COBB COUNTY FUTURE LAND USE MAP

## FUTURE LANDUSE LEGEND

- RR Rural Residential
- VLDR Very Low Density Residential
- LDR Low Density Residential
- MDR Medium Density Residential
- HDR High Density Residential
- NAC Neighborhood Activity Center
- CAC Community Activity Center
- RAC Regional Activity Center (Note: Sub-Categories)
- IC Industrial Compatible
- IND Industrial
- TCU Transportation/Communication/Utilities
- PI Public Institution
- PRC Park/Recreation/Conservation
- CTY City
- CNSV Conservation

The Future Land Use Map does not categorize any land as Undeveloped or Agriculture/Forestry; development is anticipated throughout the county.

**Source:** Cobb County Community Development, Planning Division



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## **COMMUNITY ASSESSMENT**

The Community Assessment contains the preliminary data and analysis that was used to form the foundation of the rest of the planning document. This section is broken down into four main segments: Identification of Issues and Opportunities, Identification of Existing Development Patterns, Analysis of Consistency with Quality Community Objectives, and Analysis of Supporting Data. The assessment covers a wide variety of topics that can impact decision-making.

### **Issues and Opportunities**

The new requirements by the Department of Community Affairs (DCA) requires that comprehensive plans focus on strengths and weaknesses in the community more than a compilation of reams of data. Given this new requirement, Planning Staff has compiled a list of issues and opportunities for Marietta. These lists were established through three separate avenues: data analysis, the Comprehensive Plan Steering Committee, and surveys that were distributed to the public. It is important to note that many of the issues listed below are not specific to Marietta, but are driven by market change, demographic shifts, or societal change that is occurring throughout the region and nation. As a way to facilitate the expression of the issues and opportunities, they were consolidated into larger subject areas. They are as follows:

#### **Population**

- Existence of an increasing percentage of elderly population due to demographic shifts and population change over time;
- Changing ethnic/racial make-up of Marietta;
- Income demographics of residents where there are a large quantity of upper and lower income individuals; but insufficient numbers of people with moderate incomes.

#### **Economic Development**

- High land costs and scarcity of undeveloped land make business location and expansion expensive;
- Health care and entertainment sectors are rapidly expanding industry clusters;
- Presence of Brownfield sites on older commercial and industrial properties;
- Marietta contains a variety of options for the transport of goods;
- Tourism is an important part of Marietta's retail base and image;
- Tax Allocation Districts help provide economic incentive for blighted areas;

- Marietta’s low tax rate encourages private sector investment;
- More jobs are needed that assist lower income individuals to expand into the middle income brackets;
- Communication between the public and private sectors should be improved;
- Lack of class “A” office space.

### **Housing**

- High percentage of renters and renter-occupied units in Marietta;
- The rental occupancy rate decreased from 1990 to 2000;
- Large number of public housing units;
- The city contains many stable single-family residential areas;
- Lack of high quality rental units;
- Assist low income individuals to become homeowners;
- Marietta has a diversity of housing options;
- Climate that is not conducive to change (NIMBY);
- Lack of “work force” housing;
- Lack of “senior” housing;
- Aged and deteriorating housing stock near the city center;
- Code enforcement.

### **Natural Resources**

- Abandoned or contaminated properties (Brownfield sites);
- Kennesaw Mountain and the Rottenwood Creek are important natural resources;
- Non-point source pollution in our local waterways;
- “Clustering” of housing to allow development and protect sensitive habitat;
- Specimen trees exist throughout Marietta;
- Air pollution in the Atlanta Metropolitan area;
- Insufficient greenspace;
- Reductions in tree cover citywide.

### **Cultural Resources**

- Loss of historic resources;
- Marietta has five (5) Nationally Registered Historic Districts;
- Lack of knowledge and education about local historic resources.

### **Community Facilities**

- Lack of first class community centers;
- Glover Park is an ideal public gathering space;
- Marietta School System has a state-of-the-art high school, a new math, science, and technology magnet schools, and the International Baccalaureate program to make it competitive with other metropolitan Atlanta public school systems;
- More pleasant gathering spaces needed throughout the community;
- Public infrastructure upgrades are needed in target areas to assist redevelopment;
- Perceived need for improved facilities in specific neighborhood schools;
- Lack of recreational space and opportunities for the current population size.

### **Transportation**

- Insufficient capacity on major corridors;
- Poor east-west connectivity;
- Lack of bicycle and pedestrian facilities in some areas of Marietta;
- Cobb County Transit is a quality mass transportation system;
- Insufficient parking in the downtown area;
- Public transit could serve more neighborhoods throughout Marietta.

### **Intergovernmental Coordination**

- Lack of county and regional collaboration in economic and land use policy;
- Lack of coordination between the City/Board of Lights and Water, the Marietta City School System, and the Marietta Housing Authority.

### **Land Use/Urban Design**

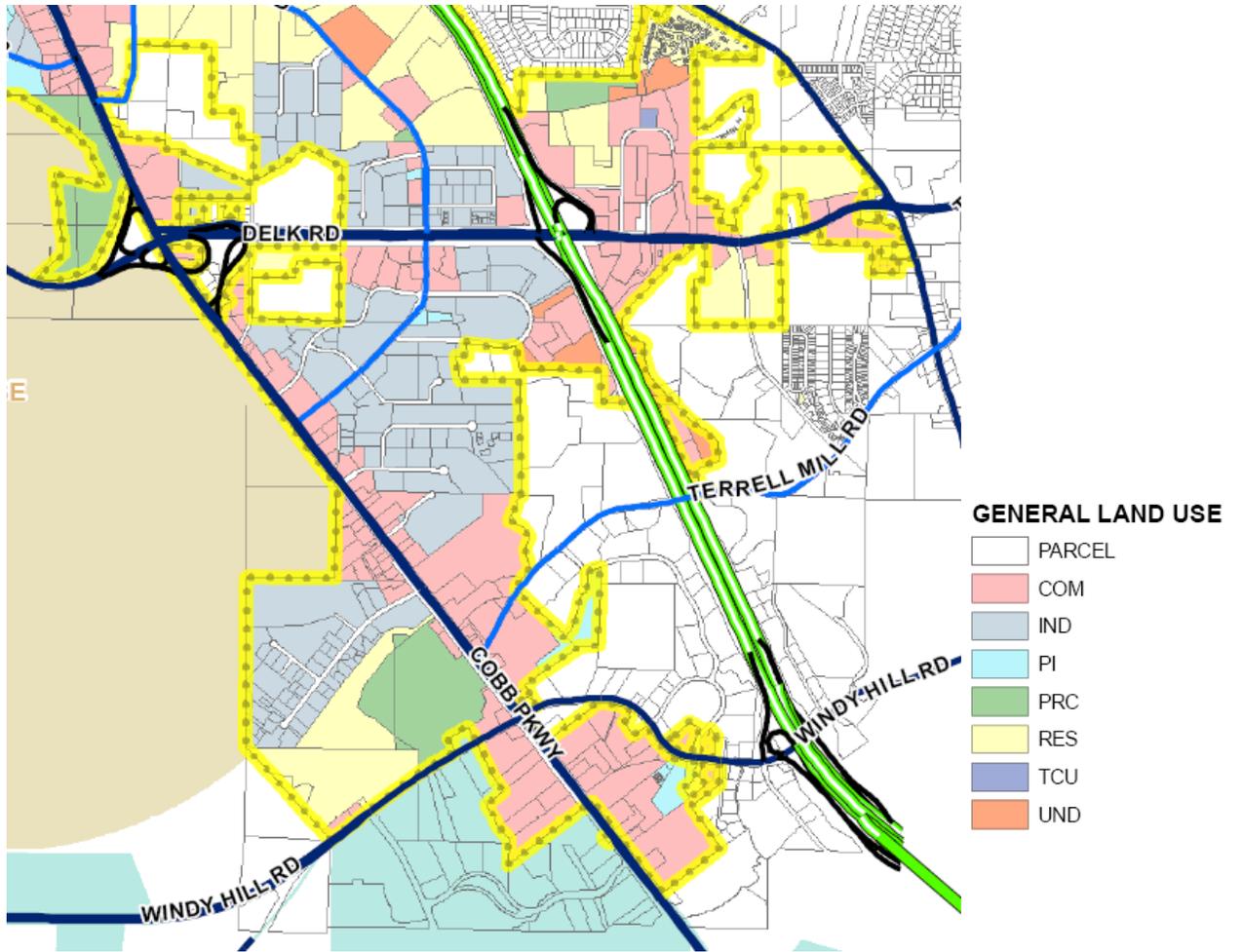
- Too much land dedicated to parking and impervious surface;
- Infill housing can provide more market-preferred housing in the center city;
- Unattractive and antiquated commercial centers;
- Marietta one of the best job-housing balance in metropolitan Atlanta;
- Visual clutter along roadways;
- Undeveloped land in the city center;
- Complaints about development approval processes in Marietta;
- Aging corridors could be rejuvenated with new investment and a mixture of uses;
- Neighborhood opposition to residential density and to innovative development ideas;

- Lack of high quality commercial development in Marietta;
- Infill housing does not blend with the existing neighborhood;
- Development in Nationally Registered Historic Districts does not occur in a manner that is sensitive to the historic architecture of the District;
- Marietta Square is a historic town center and an asset that creates a visual impression on residents and visitors alike;
- Lack of a sense-of-place along major corridors;
- Too many strip centers where parking is the only visible feature;
- Lack of identity beyond Downtown Marietta.

### **Redevelopment**

- High land cost for residential and non-residential properties;
- Proximity to Atlanta and major interstate highways will make Marietta an attractive location for new investment;
- Smaller house sizes in the city center do not meet market preferences;
- Better public relations will allow the public; business enterprises; and developers a better understanding of the incentives and opportunities in Marietta;
- Inadequate public facilities and utilities in areas needing redevelopment;
- Poor perceptions of neighborhoods requiring redevelopment;
- Absentee landlords;
- Many multi-family complexes are deteriorating and have high vacancy rates making them prime locations for private sector redevelopment;
- Crime has a negative impact on areas that should be stable;
- Prevalence of single-family detached housing in redevelopment areas will facilitate the redevelopment process.

# CITY OF MARIETTA ADJACENT LAND USE



**Source:** City of Marietta Planning and Zoning Department

## **CUMBERLAND CID MARKETING SURVEYS** **(April 2005)**

### Key Findings

- Significant untapped potential of the business and consumer wallet:  
Business visits outside the area are significant, presumably due to lack of options.  
Significant percentage of consumers would shift visits to new locations in Cumberland/Galleria Area.
  - √ *Action: Bring in retail and entertainment options that will keep business and consumers in the area and encourage residents to stay.*
  - √ *Short-term Strategy (next 5 - 10 yrs): Significant demand exists and a positioning strategy based on convenience will best leverage the area's strengths.*
  - √ *Long-term Strategy (5 - 10+ yrs): Establish Cumberland/Galleria as a destination; a unique place.*
  
- CCID initiatives have high value among Business respondents; yet, CCID is a secret.
  - √ *Action: Raise awareness about area improvements and assign credit.*
  
- More places in Atlanta where people can live, work and play makes Cumberland/Galleria area a less competitive place to locate and/or keep a business.
  - √ *Action: Ensure businesses have the opportunity to live and play where they currently work AND residents have opportunity to play and work where they live.*

## **CUMBERLAND CID FINAL MARKETING PLAN** **(July 2006)**

The Bantam Group's research determined the following key findings that were presented and agreed by the CCID.

- **Investment in the Area**

Cumberland/Galleria currently has significant untapped potential of the business and consumer wallet. This is supported by the fact that business visits outside the area are significant and is presumably due to a lack of options in the area. Also consumers have shown that should new locations open in the Cumberland/Galleria area they would shift their visit to patronize these amenities.

- √ **Indicated Action:** encourage retail and entertainment investment in the area to keep business and consumers in the area and in turn encourage residents to stay.

- **CCID Initiatives**

Although the initiatives created by the CCID are perceived to have a high value among business respondents the CCID has very low awareness among this group.

- √ **Indicated action:** raise awareness of the area improvements to the business community and assign credit.

- **Competition**

More places in Atlanta where people can live, work and play makes Cumberland/Galleria a less competitive place to locate and/or keep a business, and to position the area as a destination.

- √ **Indicated Action:** Encourage investment in the area in the long-term to ensure that businesses have the opportunity to live and play where they currently work, and that residents have the opportunity to play and work where they currently live.

- **Create awareness and develop a foundation for the perception that Cumberland/Galleria is a desirable place to live, work and play, focusing on the successes of the Cumberland CID.**
  - √ Continued enhancement and protection of property values;
  - √ Re-affirming to the constituents that investing in Cumberland/Galleria is a profitable choice;
  - √ Keeping and attracting businesses to the area;
  - √ Encouraging to the extent currently possible, those who live and work in the area to stay and spend.
  - √ Create a Visual Identity
  - √ Create a dedicated CCID business oriented web location; a Consumer Friendly Website
  - √ Develop and Execute Direct Communications
  - √ Create Business-to-Business Print
  - √ Special Events
  - √ Establish the “Communication Infrastructure” for consumers
  - √ Name the Area
  - √ Expand Community Relations
  - √ Loyalty Program for the CCID to reduce competition from other CIDs

## **BLUEPRINT CUMBERLAND II VISIONING PROCESS**

In July 2006, the stakeholders for the Blueprint Cumberland area were divided into nominal groups to share their views concerning the current state of the area and their vision and goals for its future. The stakeholders were guided through a SWOT analysis to develop the strengths, weaknesses, opportunities and threats for the Blueprint Cumberland area today. After completing that task, the groups crafted ideas for how they prefer the area to be in 10 years.

### **Cumberland/Galleria Today**

#### **Strengths**

- + Location
- + Access
- + Strong Leadership
- + Strong Infrastructure
- + Low Taxes
- + Redevelopment Plans already in place
- + Greenspace
- + Identity

#### **Weaknesses**

- Aging rental properties
- Lack of adequate neighborhood shopping opportunities
- School system is inadequate
- Employment/Residential ratio
- Lack of transportation options
- Congested interstate system

#### **Threats**

- ? Sandy Springs expansion
- ? Future of Dobbins AFB
- ? Perception of area
- ? Image of Cumberland Mall
- ? Highway expansion
- ? Perception of Crime

### **Cumberland/Galleria in 10 Years**

- √ Neighborhood churches, stores & schools
- √ Affordable housing
- √ Senior housing
- √ Mixed-use development
- √ Parks and Recreation, more greenspace
- √ Shuttle service within the area
- √ Strong marketing campaign
- √ Sense of community
- √ Town Center or focal point

# **BLUEPRINT CUMBERLAND II CHALLENGES**

## **Core Area Challenges**

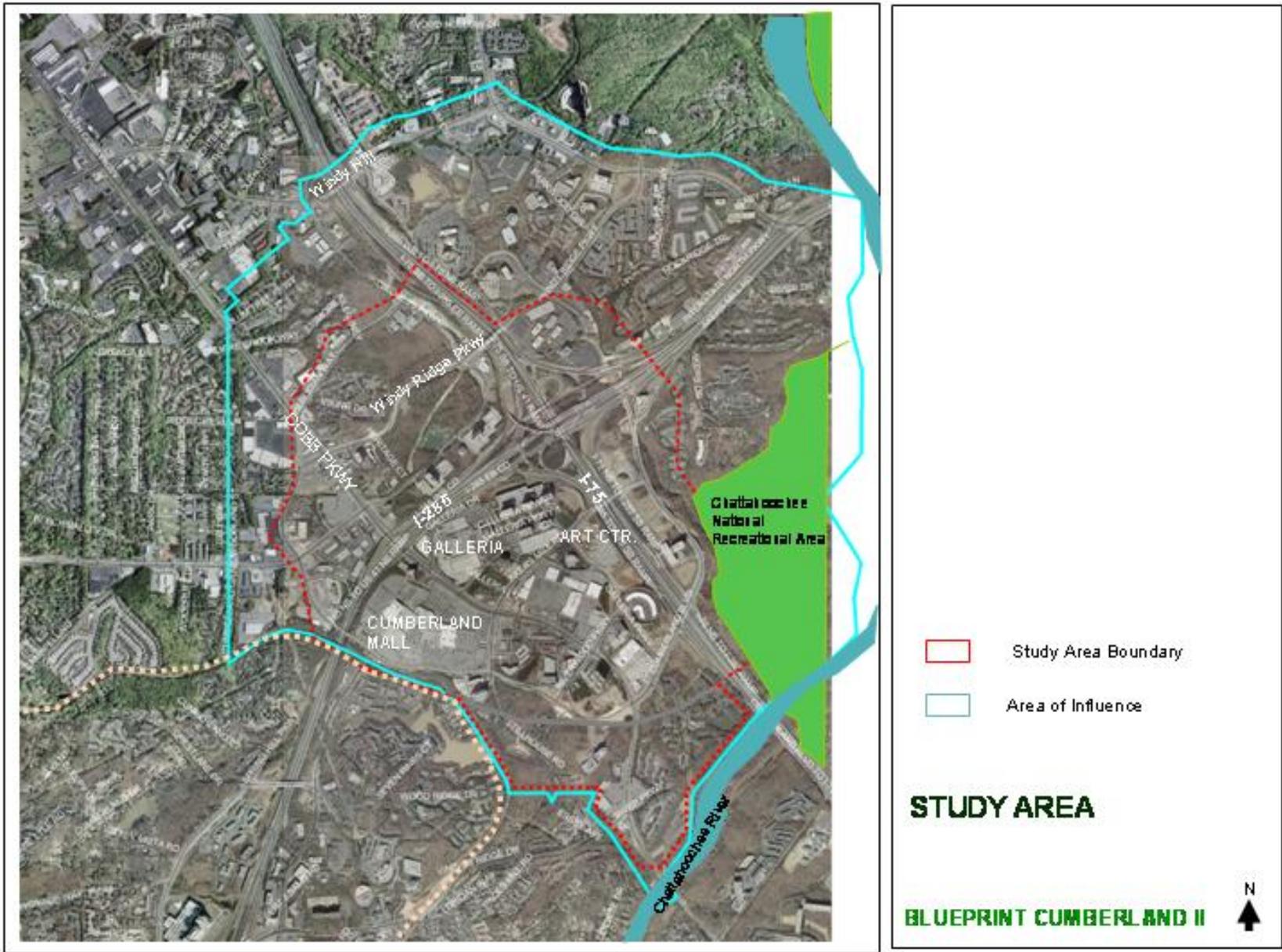
- Auto-Oriented Development
- Scale – Mega Blocks, not pedestrian-friendly
- Mix of Uses – limited to retail and office
- Jobs-to-housing Balance
- Underutilized Chattahoochee National Recreation Area and the River
- Underutilized Properties – aging strip malls
- Design Quality of Retail Development and Choices
- Sense of Scale and Urban Definitions: nodes, public spaces, gateways, sense of place and orientation
- Public Realm – Plazas, Open Spaces, Parks, Interactive Spaces, Focal Points
- Lacks Pedestrian Opportunities/Experience
- Trees and Streetscapes
- Bike Paths and Connectivity
- Topography and Freeways to negotiate
- Image/Identity – thematic urban character

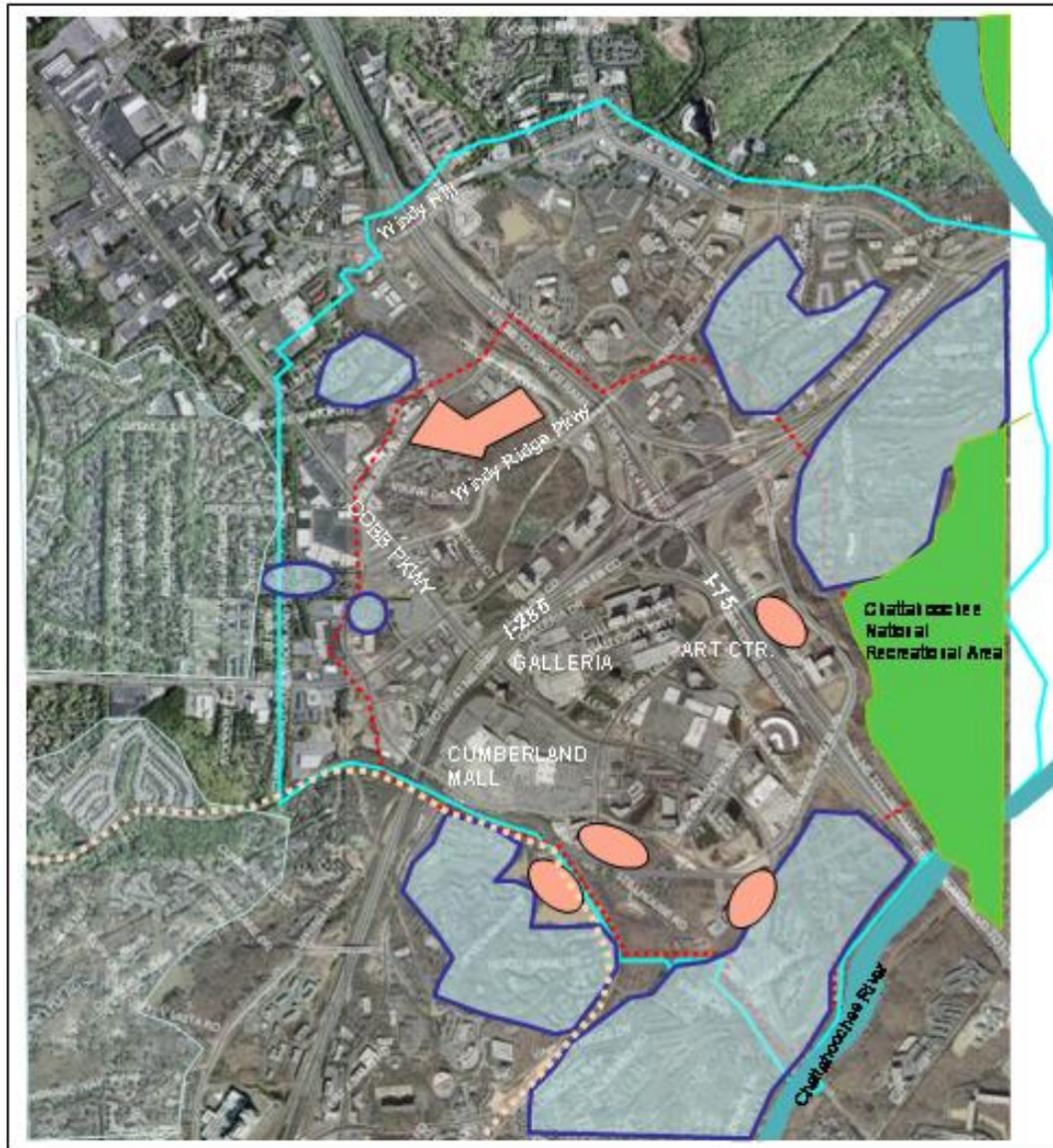
## **Area of Influence Challenges**

- Auto-Oriented Development
- Scale – Superblocks, not pedestrian-friendly
- Under-Utilized Properties - Aging and Obsolete Strip Retail
- Inter-Parcel Connectivity
- Quality of Retail and Choices
- Sense of Scale and Urban Definitions: nodes, public spaces, gateways, sense of place and orientation
- Public Realm – Plazas, Open Spaces, Parks, Interactive Spaces, Focal Points
- Lacks Pedestrian Opportunities/Experience
- Trees and Landscaping/Streetscapes
- Bike Paths and Connectivity
- Abundance of Apartments – of all levels and quality

## **Blueprint Cumberland II Master Plan Concepts**

- Identify the location of the Heart/Focal Point and critical mass of activities
- Transit Station
- Hierarchy of Activity Nodes within walkable radius – hotel/conference/motel concentrations and Apartments/Offices
- Choice of activities – retail types, dining, recreational and cultural – identify locations – Lifestyle
- Scale of Streets – Public Realm/Experience – big boxes/underutilized parcels
- Multi-Path Connections
- Identify new housing – luxury condos/townhomes – walkable and connection to trail/river
- Identify location of pocket parks/public spaces in conjunction with “nodes”



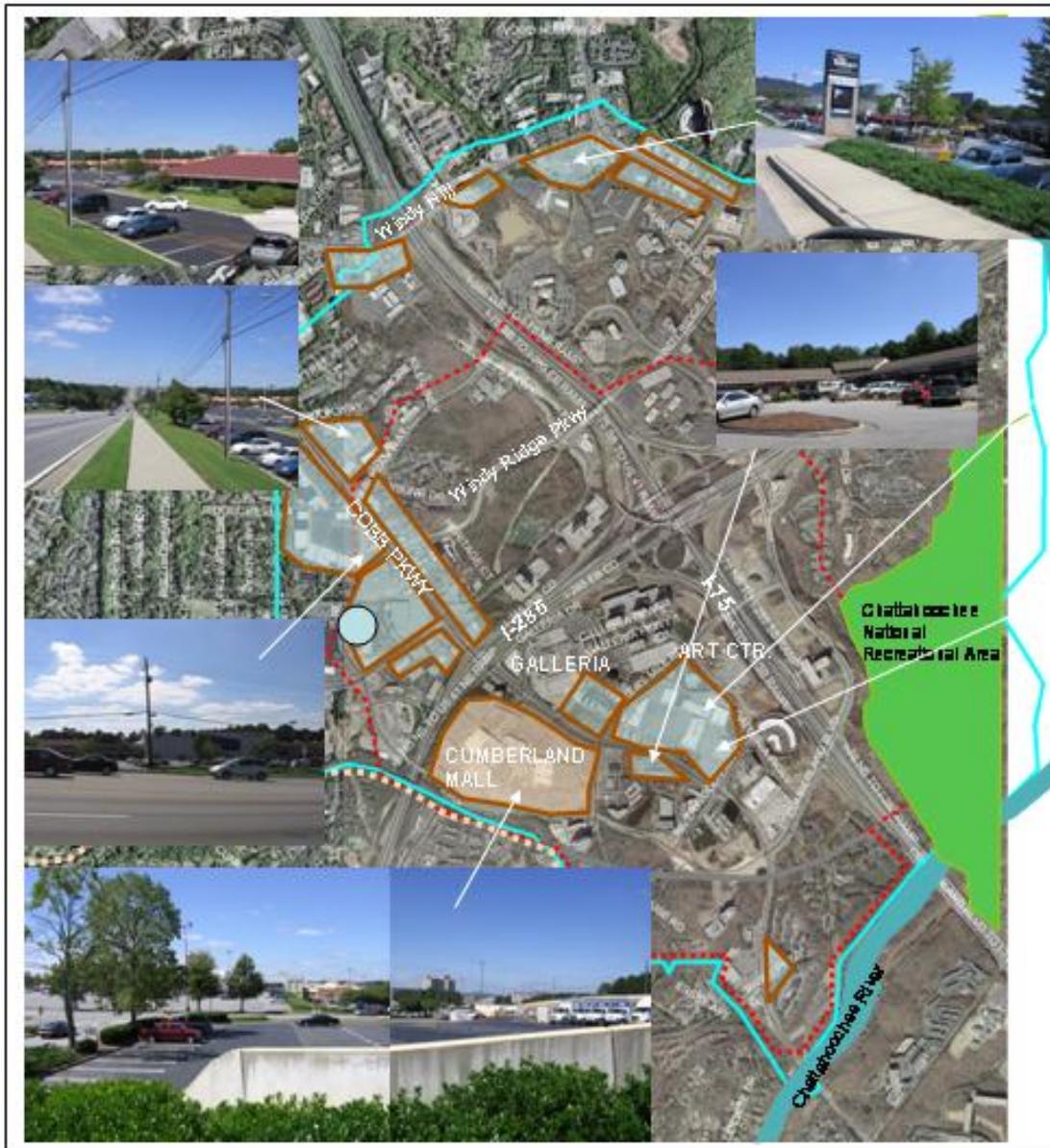


- Housing - Apartments
- Housing - Town Homes/ Luxury Condos (proposed)

**RESIDENTIAL**  
 (Supplementary housing study, 2003)

**BLUEPRINT CUMBERLAND II**

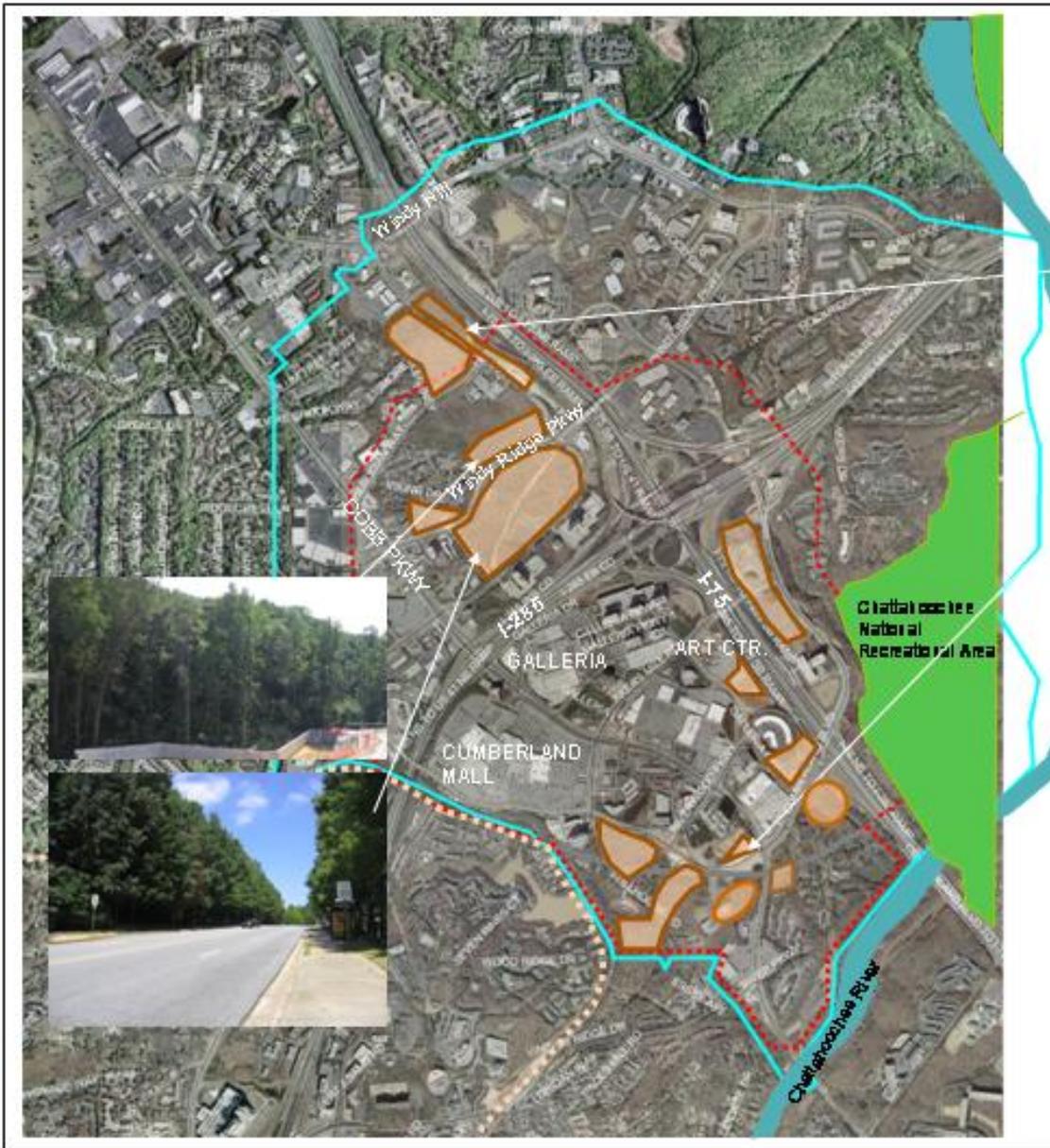




-  Redevelopment Potential of Retail
-  Redevelopment currently underway – integration/additional opportunities

**REDEVELOPMENT OPPORTUNITIES**

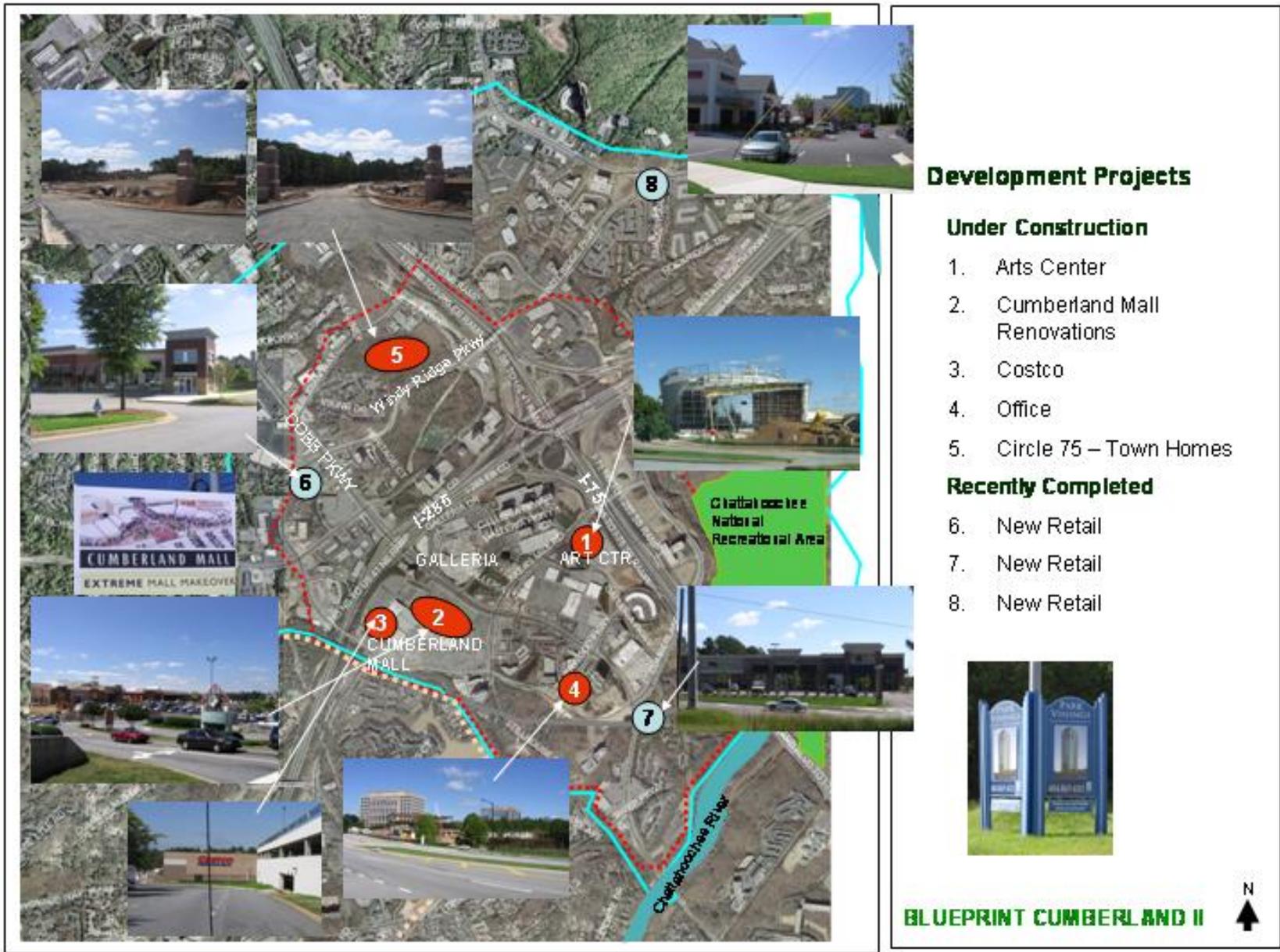
**BLUEPRINT CUMBERLAND II** 



**VACANT  
PROPERTIES  
(2006)**

**BLUEPRINT CUMBERLAND II**





**Development Projects**

**Under Construction**

- 1. Arts Center
- 2. Cumberland Mall Renovations
- 3. Costco
- 4. Office
- 5. Circle 75 – Town Homes

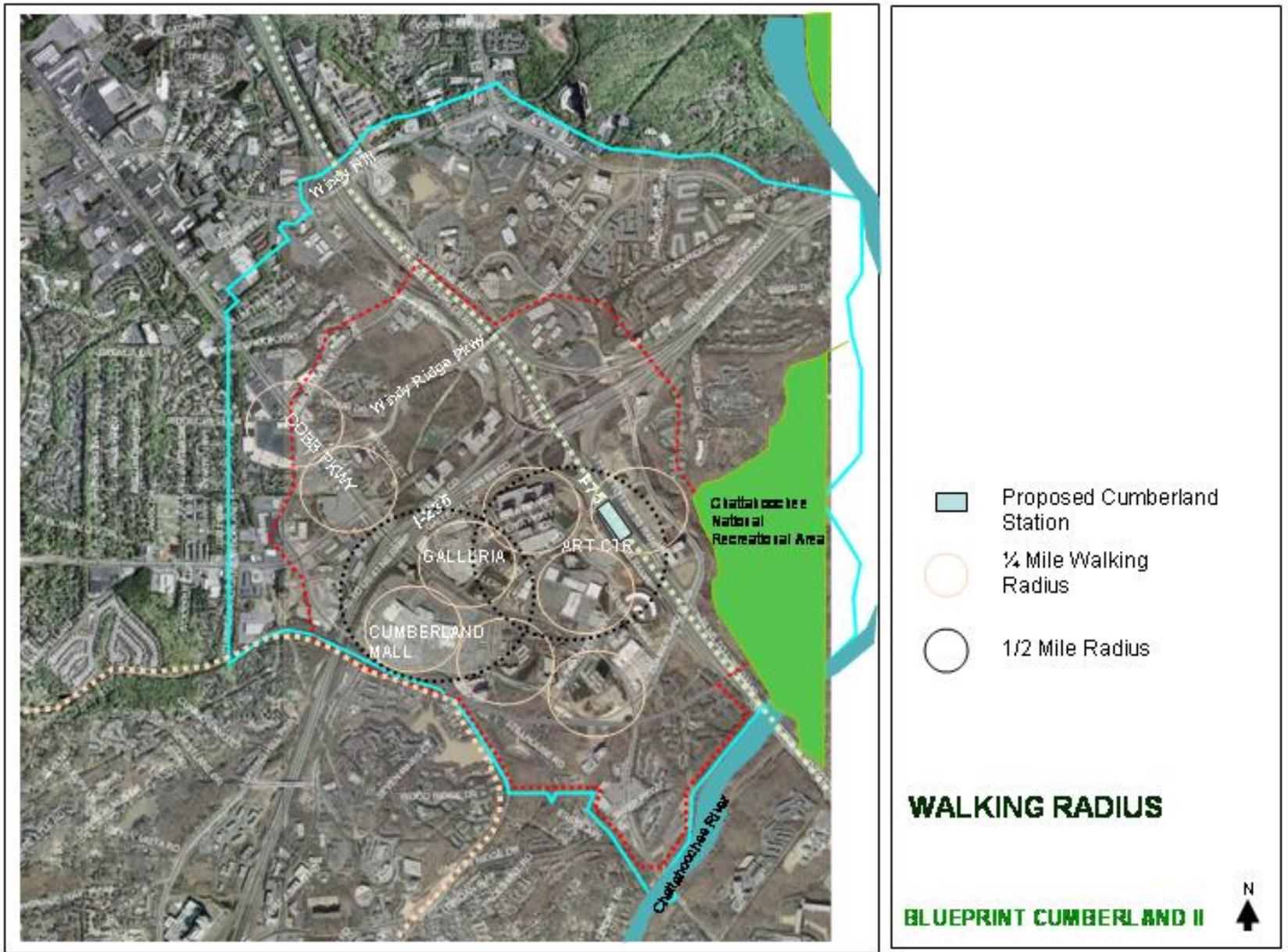
**Recently Completed**

- 6. New Retail
- 7. New Retail
- 8. New Retail



**BLUEPRINT CUMBERLAND II**





# **STEPS FOR MASTER PLANNING-CASE STUDIES**

## **1. Understand Your Position in the Market**

- √ Competing districts
- √ Differentiation
- √ Market potential

## **2. Build Community Support**

- √ Three-Way team (Government, Private, Residents)
- √ Process of open participation
- √ Keep everyone informed
- √ Simplify information and approvals

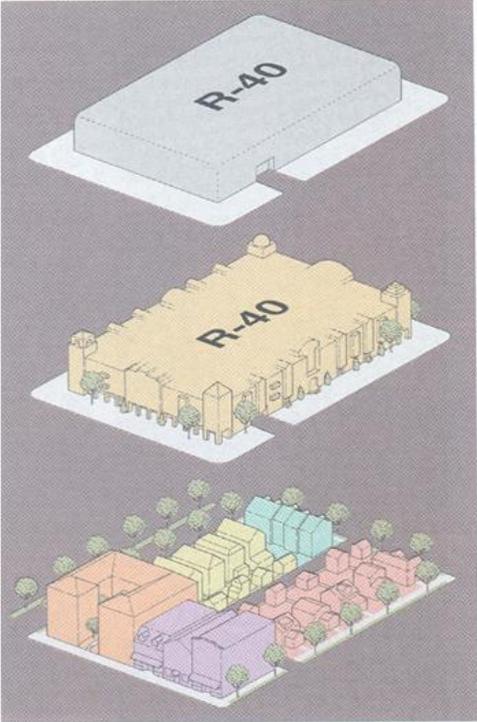
## **3. Craft a Vision and a Plan**

- √ With design, economics and aspirations
- √ Innovative and realism
- √ Supported by the stakeholders
- √ Strategic plan
- √ Monitor master plan
- √ Stakeholder commitment to master plan

## **4. Results over Regulation**

- √ Focus on place-making and form not zoning
- √ Revise zoning to create places rather than uses
- √ Use virtual tools

Results over Regulations



Flexibility

Source: ULI



Visualization to go beyond zoning to place making



**Downtown Kendall, Miami, FL**  
*From thriving mall and office park to downtown  
Transit-oriented-development*  
Dover, Kohl & Partners, Duany Plater-Zyberk  
& Company

Source: Ellen Dunham Jones, Georgia Tech

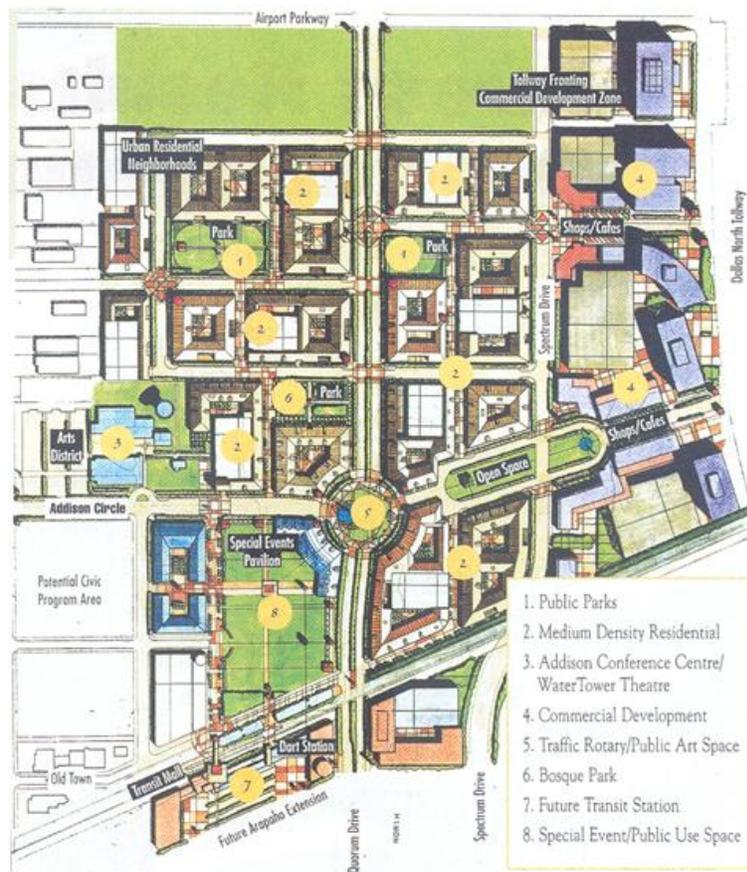
## **5. Optimize Connectivity/Grid Patterns over Superblocks**

- √ Re-orient roads-pedestrian connectivity
- √ Enhance public spaces
- √ Define civic/community uses
- √ Developed structured parking
- √ Public-private partnerships
- √ Transit before demand
- √ Financing alternatives

## **6. Embrace Mixed-Use**

- √ Remove barriers and parking requirements
- √ Provide incentives – public parking
- √ Pedestrian access to multiple uses
- √ Cluster interconnected activities
- √ Integrate community infrastructure

## Vision and a Plan



Source: ULI

## Embrace Mixed Use



Source: ULI



**Belmar, Lakewood, CO**  
*From dead mall to town center*  
**Continuum Partners**  
**Elkus Manfredi**  
**Van Meter Williams Pollack**



Upper Rock -2 (before and after)



Source: Ellen Dunham Jones, Georgia Tech

## **7. Honor the Human Scale/Create Pedestrian - Friendly Places**

- √ Continuity of street-level activity
- √ Art, architecture and icons
- √ Provide choices for transportation
- √ Safe age, diverse community life
- √ Public parks with cultural events

## **8. Think Transit/Think Density**

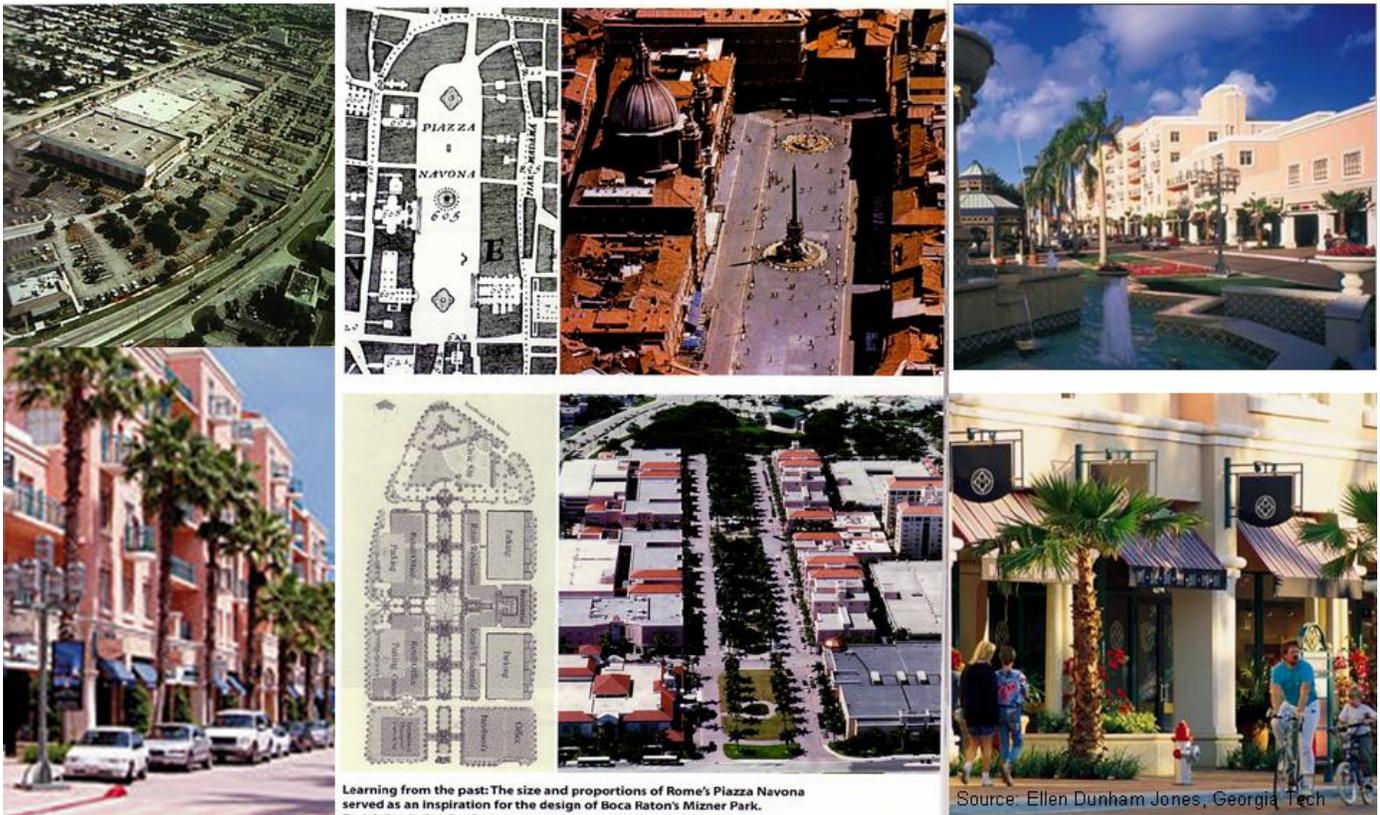
- √ Provide ancillary activities
- √ Increase density-connectivity
- √ Enhance transportation choice

## Human Scale and Pedestrian Friendly



Source: ULI

**Mizner Park, Boca Raton, FL; Cooper Carry Architect, 1990**  
*From dead mall to mixed-use downtown*



## Break Up Super Blocks



Source: ULI

# THINK transit



# Icons



Source: Sizemore Group

# Icons

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Source: Sizemore Group

## **9. Forge Public-Private Partnerships**

- √ Fairness to all stakeholders
- √ Leverage private capital
- √ Public investment in pedestrian connectivity and places
- √ Harness all public resources to implement master plan

## **10. Share and Manage Parking**

- √ With street level activity
- √ Charge nominal fees for space rental
- √ Purchase spaces from public deck

## Share and Manage Parking



Source: ULI



📍 *Downtown Kendall, Florida*  
*From Edge City to Suburban Downtown*

Rebuilt the old Dadeland Mall originally constructed in 1962. Transformed into a city of 400,000 residences over the next three decade. The redevelopment of the 324-acre Dadeland Mall includes tree-lined streets, grid patterns by breaking-up existing superblocks, and linear buildings around big box blank walls. The existing polluted canal was restored with a linear public park with fronting mixed-use.



**Downtown Kendall, Miami, FL**  
*From thriving mall and office park to downtown*  
*Transit-oriented-development*  
Dover, Kohl & Partners, Duany Plater-Zyberk & Company

## Kendall - 2

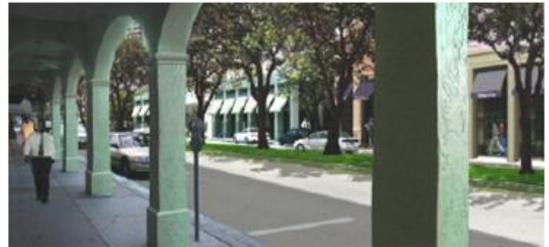
Town Square: the heart of the new Kendall community, overlooked by multi-story mixed-use and civic buildings



Snapper Creek Canal: Before and After



Dadeland Boulevard: Before and After



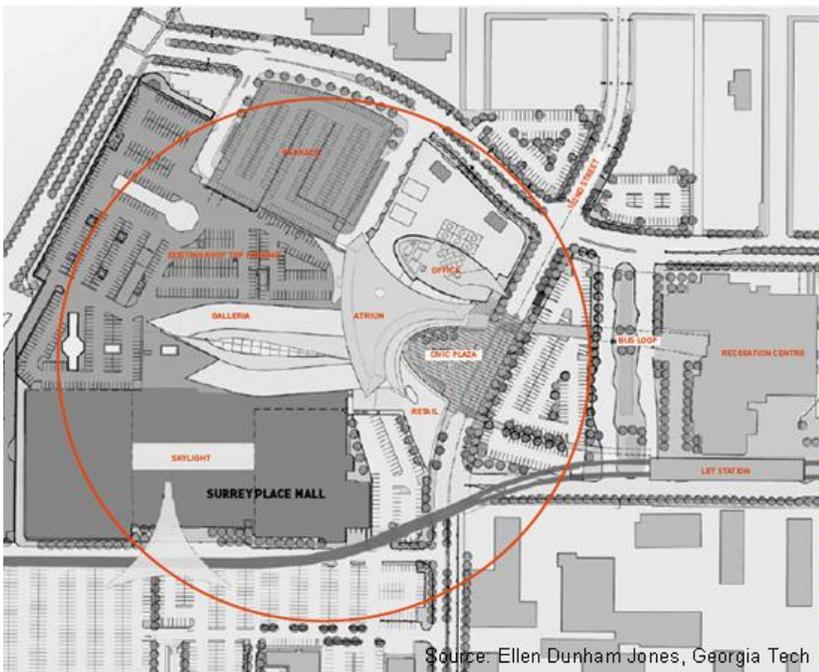
construction images

Source: Ellen Dunham Jones, Georgia Tech

● *Surrey Central City, British Columbia*  
*From Mall to Transit-Served University and Office Tower*

The redevelopment site is located about 45 minutes outside Vancouver on a stop for the new Light Rail “Sky Train.” The development includes a new 25 story office tower and five story Galleria above the existing Mall. The upper levels of the Mall include the new Simon Fraser University. The project was constructed while the existing Mall remained open.

**Surrey Central City, Surrey, (Vancouver) British, Columbia**  
*From dated mall to mall + university + office tower + transit*  
*focused on central plaza and atrium*  
Bing Thom Architects, inc



Source: Ellen Dunham Jones, Georgia Tech



## **MASTER PLANNING PROCESS**

Studies of this nature involve many participants and stakeholders, both on the client side as well as the consultant's side. For a successful planning study, it is imperative to hear all of the voices and integrate them in the planning process. Stakeholder input was solicited at various stages during the planning study. The Master Plan was developed through broad stakeholder input, review and visioning and the analysis of past studies and the potential of the CCID study area. Following are the methodologies and stakeholder participation mechanisms utilized through the planning study:

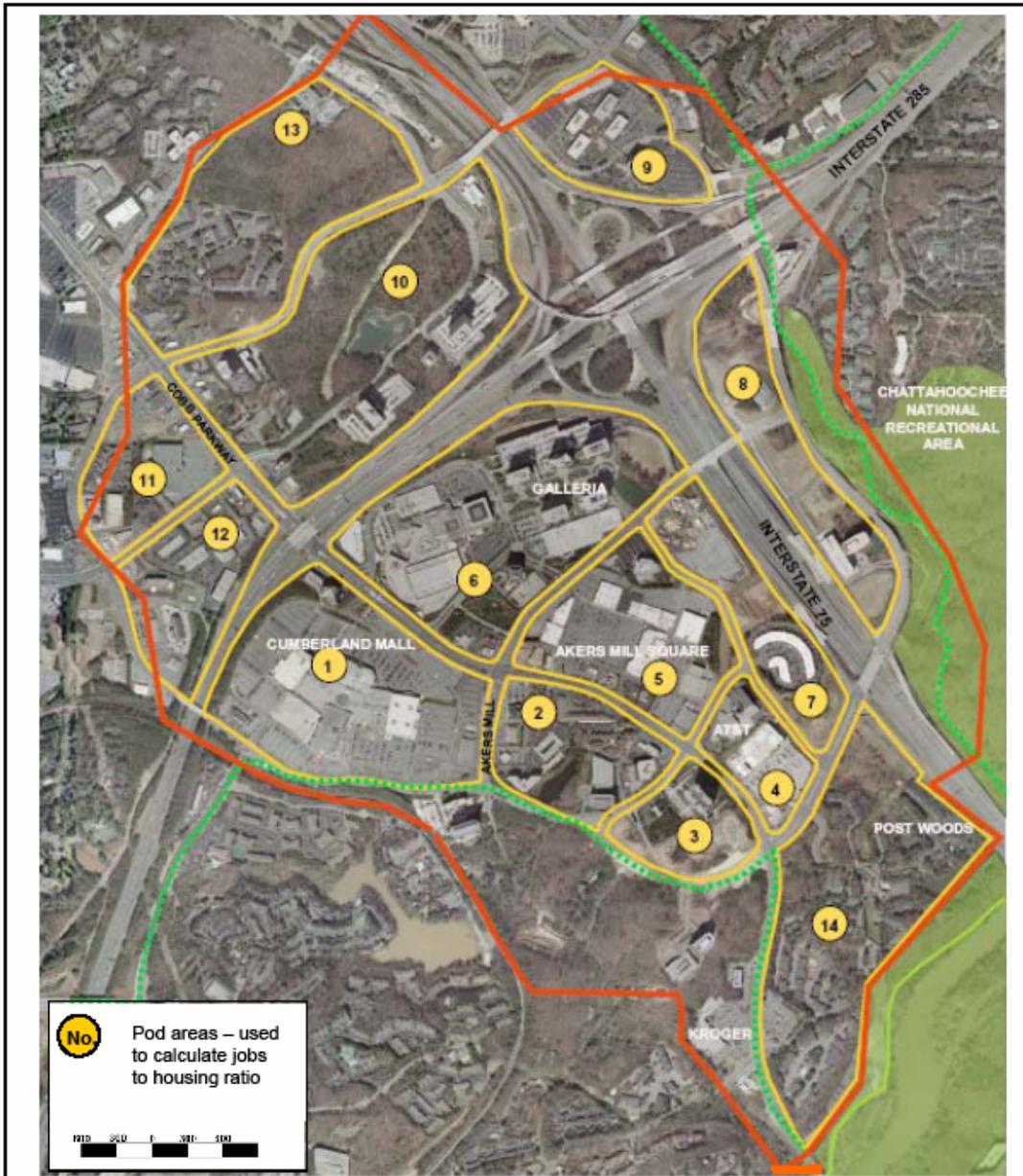
- **Stakeholder Input:** The consultant team conducted meetings with stakeholders to solicit their input to understand the issues, opportunities and aspirations. This facilitated a very good understanding of the Cumberland/Galleria Area. The stakeholders included Cobb County, City of Marietta, GDOT, ARC, CCID and its members, business owners, and developers.
- **Analysis and Review of Past Studies:** As part of the Blueprint Cumberland II planning effort, the project team reviewed a variety of existing documents and initiatives; Blueprint Cumberland I study, Northwest Corridor Study-HOV/BRT, Cumberland LCI Supplemental Housing Study, Southeast Quadrant Study, Cobb County's Comprehensive Plan and Comprehensive Transportation Plan, urban design standards, future land use, zoning, aerial photographs, and regional transportation initiatives. Windshield field surveys were conducted to analyze/verify the existing conditions: land use, building conditions, design character, transportation issues, pedestrian and vehicular circulation, safety and others. The analysis of existing conditions and market studies supplemented the issues and the opportunities analysis in the Cumberland/Galleria area
- **Master Plan Workshop:** A stakeholder workshop was held on March 14th, 2007 for the planning team to present three alternative master plans for the study area. The stakeholders were asked to review the alternatives and discuss the pros and cons of each alternative. The attendees were divided into three groups; each group reviewed one alternative and presented the pros and cons of that alternative to the entire group. The stakeholders reached consensus on some aspects in each concept. The plans were then revised from the consensus of opinions from the stakeholders meeting as a single group.

Based on the community's vision for the study area, analysis of the study area and its precincts, development opportunities and market demand in the area, the planning team prepared a concept designed to leverage the resources of the study area, encourage development and redevelopment and promote a variety of uses including retail, entertainment and residential uses in the Cumberland/Galleria area currently dominated by office uses.

The following key principles guide the Master Plan Scenarios:

- Leverage existing assets such as the Cumberland Mall, the new Cobb Energy Performing Arts Centre, Cobb Galleria, proposed transit station and the Chattahoochee National Recreation Area.
- Increase accessibility, easy circulation and walkability within the Cumberland/Galleria area by providing a finer-grained network of pedestrian-friendly streets, multi-use paths and trails and also by promoting use of transit in the area.
- Transform the Cumberland/Galleria area from office/retail that generates only daytime activity to a 24/7 mixed-use lifestyle activity center that supports a live-work-play environment with a distinct architectural character and sense of place.
- Consolidate the retail and commercial activities along Cobb Parkway into a cohesive commercial district that will allow for an array of commercial/retail uses and activities.
- Enhance the visual quality and character of the existing arterials (parking, utilities, signage, streetscape and artwork)
- Address the transportation and pedestrian safety issues along Cumberland Boulevard, Cobb Parkway and Akers Mill Road: ingress/egress, curb cuts, intersection improvements, traffic calming, inter-connectivity of parcels, street grids, median cuts, transit integration and access management.
- Promote residential mixed-use development along Windy Ridge Parkway in line with the existing and proposed pattern of development.
- Provide diverse housing types and product mix that promotes economic, social and cultural mix and residential neighborhoods.

- Promote a balanced 'Jobs-to-housing Ratio' in the Cumberland/Galleria area. Currently the Cumberland/Galleria Area is primarily an office district with an imbalanced jobs-to-housing ratio; there is a shortage of housing in the vicinity of work places, causing people to commute longer distances for work. This leads to heavy congestion and deteriorating air quality.





**CROY**  
ENGINEERING

Engineers  
Planners  
Surveyors

**CUMBERLAND CORE AREA**  
DEVELOPMENT PODS



NORTH



CUMBERLAND  
COUNCIL OF GOVERNMENT DISTRICT

Increasing diversity of uses and activities; art/recreation, employment, living, shopping, convention/hotel and access to the Chattahoochee National Recreation Area will place the Cumberland/Galleria area ahead of the curve from other office districts in the Metro-Atlanta area, and will help establish its position as a 24/7 live-work-play district.

The following table depicts the existing jobs-to-housing ratio within each quadrant in the Cumberland/Galleria Area. Note: This table does not include those housing units in currently approved site plans or zoning using units for the Blueprint Cumberland II Core area. The table shows the employment projections for the existing uses and square footage within the Cumberland/Galleria Core Area as shown in the map. There are a total of 470 residential units for a total of 30,279 jobs within the Cumberland/Galleria Core area. With a ratio of 64:1 (64 jobs per 1 housing unit), there is a significant deficit in the number of housing units. This reflects the large jobs-to-housing imbalance within the Cumberland/Galleria Core Area and the potential for additional housing. However, there have been a number of developments approved through site plan approval and/or zoning approval that have dramatically improved the jobs-to-housing ratio. The Cumberland CID has made outstanding progress in this area as a result of its Blueprint Cumberland planning initiative.

## Existing Development Jobs: Housing Balance

### EMPLOYMENT PROJECTIONS AND HOUSING UNITS FOR EXISTING AREAS

QUAD	NAME	RETAIL ( \$Q FT.)	JOBS (\$450F/JOB)	OFF. ( \$Q FT.) (PROPOSED)	JOBS (210 SF /JOB)	HOTEL ( \$Q FT)	JOBS 10,000 SF /JOB)	TOTAL JOBS	RES UNITS
1	CUMBERLAND MALL	615440	661						
	<b>TOTAL</b>	<b>616440</b>	<b>661</b>					<b>661</b>	
2	CUMBERLAND FESTIVAL RETAIL	126903	134						
	BARNES AND NOBLES	25117	27						
	WACHOVIA			100,000	476				
	FIRST UNION			12371	59				
	BANK OF AMERICA			7609	36				
	SPRINT CUMBERLAND CENTER I			185000	881				
	CUMBERLAND CENTER IV			223500	1064				
	OFFICE BUILDING			250488	1193				
	HOTEL						15		
	<b>TOTAL</b>	<b>162020</b>	<b>181</b>	<b>778868</b>	<b>3709</b>		<b>16</b>	<b>3886</b>	
3	RIVERWOOD COMPLEX			500000	2381				
	<b>TOTAL</b>			<b>600000</b>	<b>2381</b>			<b>2381</b>	
4	PARKWAY POINT THEATER	113170	120						
	DON PABLOS RESTAURANT	15447	16						
	AT&T			374764	1785				
	<b>TOTAL</b>	<b>128617</b>	<b>136</b>	<b>374764</b>	<b>1786</b>			<b>1921</b>	
6	AKERS MILL SQUARE RETAIL DISTRICT	397776	421						
	<b>TOTAL</b>	<b>397776</b>	<b>421</b>					<b>421</b>	
8	GALLERIA MALL	637453	675						
	RENAISSANCE HOTEL					5661119	566		
	700 GALLERIA			430300	2049				
	100 GALLERIA			410571	1955				
	400 GALLERIA			440000	2095				
	300 GALLERIA			433366	2064				
	200 GALLERIA			432698	2060				
	MEN'S WAREHOUSE	6255	7						
	<b>TOTAL</b>	<b>643708</b>	<b>681</b>	<b>2148936</b>	<b>10224</b>	<b>6661119</b>	<b>688</b>	<b>11471</b>	
7	75 GALLERIA			113000	538				
	<b>TOTAL</b>			<b>113000</b>	<b>688</b>			<b>688</b>	
8	EXISTING OFFICE BLDG			415181	1977				
	<b>TOTAL</b>			<b>415181</b>	<b>1977</b>			<b>1977</b>	
9	THE WEATHER CHANNEL			168616	803				
	320 BUILDING			113540	541				
	340 BUILDING			90841	433				
	360 BUILDING			145732	694				
	380 BUILDING			90493	431				
	<b>TOTAL</b>			<b>609222</b>	<b>2801</b>			<b>2801</b>	
10	1100 BUILDING			256000	1219				
	900 BUILDING			340000	1619				
	SAUL REAL ESTATE			94080	448				
	HAMPTON SUITES HOTEL					72424	7		
	OFFICE BUILDING			13635	65				
	DOUBLE TREE SUITES ( 155 ROOMS)					127470	13		
	JADE PALACE	7142	8						
	DUNKIN DONUTS	3690	4						
	JUST BRAKES	6001	6						
	<b>TOTAL</b>	<b>16833</b>	<b>18</b>	<b>703716</b>	<b>3361</b>	<b>189894</b>	<b>20</b>	<b>3389</b>	
11	RESIDENCE INN (130 ROOMS)			49242	234				
	STORAGE								
	QT								
	ARBY'S	4850	5						
	GOLF SHOP	8728	9						
	SERVICE MERCHANDISE	60900							
	<b>TOTAL</b>	<b>74478</b>	<b>14</b>	<b>49242</b>	<b>234</b>		<b>12</b>	<b>281</b>	
12	EMERSON CENTER	17732	19						
	EMERSON CENTER OFFICE PARK			74134	353				
	OFFICE BUILDING			11000	52				
	HOLIDAY INN ( 144 ROOMS)								
	SUMNER SUITES( 125 ROOMS)								
	SHOPS AT CUMBERLAND PLACE	33200	36						
	APPLBEE'S	7336	8						
	WENDY'S	3188	3						
	PIZZA HUT	3668	4						
	RETAIL MISC	8517	9						
	<b>TOTAL</b>	<b>73842</b>	<b>78</b>	<b>85134</b>	<b>406</b>			<b>483</b>	
13	RESIDENTIAL DEV.								70
	<b>TOTAL</b>								<b>70</b>
14	RESIDENTIAL DEV.								400
	<b>TOTAL</b>							<b>0</b>	<b>400</b>
	<b>TOTAL</b>							<b>30279</b>	<b>470</b>

Jobs to Housing Ratio: 1 job per 64 housing units

The table below provides the number of proposed jobs and housing units by quad for those developments currently approved through site plan and/or zoning.

QUAD	NEW JOBS	NEW HOUSING UNITS
1	81	0
2	0	0
3	2558	856
4	0	0
5	0	0
6	126	250
7	1955	400
8	0	200
9	0	0
10	0	0
11	0	0
12	0	0
13	0	1534
14	714	0
15	0	770
<b>TOTAL PROPOSED</b>	<b>5434 jobs</b>	<b>4010 housing units</b>

Note: Based on currently approved zoning and site plan approvals in the Blueprint II Core area.

The revised jobs-to-housing ratio is displayed in the table below. By including the proposed developments, the ratio improves from 64:1 to 8:1. An ideal ratio for the Cumberland CID would be 4 jobs for every housing unit.

Category	Quantity
Current Employment	30,279
Current Housing Units	470
Current Jobs-to-housing Ratio	64:1
Potential New Jobs*	5,434
Potential New Housing Units*	4,010
—	
Revised Employment (Existing + Proposed)	35,713
Revised Housing Units (Existing + Proposed)	4,480
Revised Jobs-to-housing Ratio	8:1
Ideal Jobs-to-housing Ratio	4:1

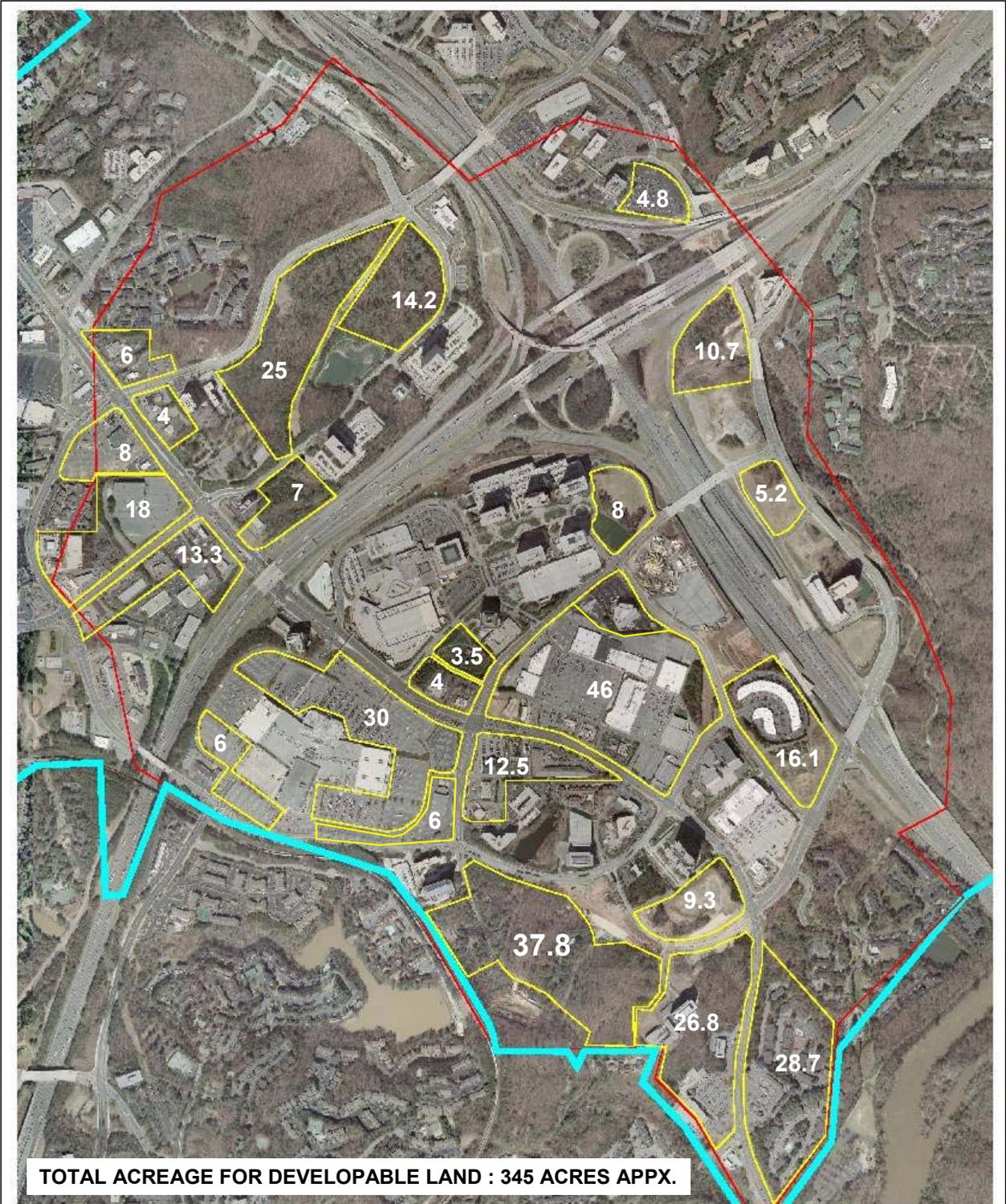
\* Note: Based on currently approved zoning and site plan approvals in the Blueprint II Core area.

### Developable Areas

Once the premise of the Master Plan was established, the planning team identified developable/redevelopable land within the Cumberland/Galleria area. The following types of land uses were considered ripe for redevelopment other than available vacant parcels:

- Single-storied strip retail
- Surface parking lots
- Aged office/retail and apartment buildings
- Other incompatible uses in the area (auto repair shops etc)

The following map shows the total developable land area of 345 acres (29.4%) of the available 1,174 total acres within the Cumberland/Galleria Core Area.



**CUMBERLAND CORE AREA – DEVELOPABLE/ REDEVELOPABLE AREAS**

## **DRAFT MASTER PLAN SCENARIOS**

Using the above guiding principles, three alternative Master Plans were produced. These alternatives explored various options possible to address the above mentioned issues and concerns and reflect the goals for the Cumberland/Galleria Area.

Once the planning team came up with recommendations and alternative concepts for the study area based on the stakeholder's input, the alternatives were presented to the stakeholders in the form of a design workshop. The event was held on March 14, 2007 at the Cobb County Chamber of Commerce. The intent of the design workshop was to solicit stakeholder input on the proposed alternatives.

The planning team presented design recommendations and three optional concept plans for the study area. The stakeholders were asked to review the options and discuss the pros and cons of each option. The attendees were broken into three groups; each group reviewed one option and presented the pros and cons of that option to the entire group. The stakeholders together reached a consensus on some aspects in each option.

The process was designed as a consensus building exercise, the stakeholders from all different teams agreed upon certain aspects of the concept plan that were common to all the three options. Some aspects of one optional plan were more accepted than the others. At the end of the workshop, the positive aspects of the three optional plans were incorporated in the final concept plan.

## Master Plan common themes – Options A, B, & C

- Increased density and diversity of uses and activities
- Redevelopment of Akers Mill Square into a mixed-use district with pocket parks and courtyards with on-street parking on the local streets
- Establishing a street grid having small walkable blocks and pedestrian-oriented streetscape
- Linear retail buildings along the periphery of Cumberland Mall that relate to the street and hide parking lots
- Pedestrian connectivity and linkage of all the major activities: Cumberland Mall, Cobb Galleria, Cobb Energy Performing Arts Centre, proposed transit, Chattahoochee National Recreational Area, entertainment uses and various proposed projects
- Linear buildings along Cobb Parkway north of I-285 and a mixed-use development that ties into the proposed Circle 75 mixed-use project with pocket parks and green/open space
- Integrating the proposed development projects and infrastructure initiatives into the Master Plan
- Jobs-to-housing balance: creating housing in each quadrant with pocket parks
- Connectivity to Chattahoochee National Recreation Area with a trailhead by the proposed TOD development
- Optimizing the terrain for great views
- Streetscape improvements and traffic calming devices at critical intersections

## Option A – Salient Features

## Additional Features that differ from other Master Plan options:

### STAKEHOLDER COMMENTS

#### PROS:

STREETSCAPES AND NEW STREETS CONNECTING EXISTING ASSETS AND ACTIVITY CENTERS

TRAFFIC CALMING MEASURES ON ALL MAJOR INTERSECTIONS

CONNECTIVITY TO EXISTING TRAIL NETWORK

TOD AROUND transit STATION AND OTHER ADJACENT DEVELOPEMNTS

#### CONS:

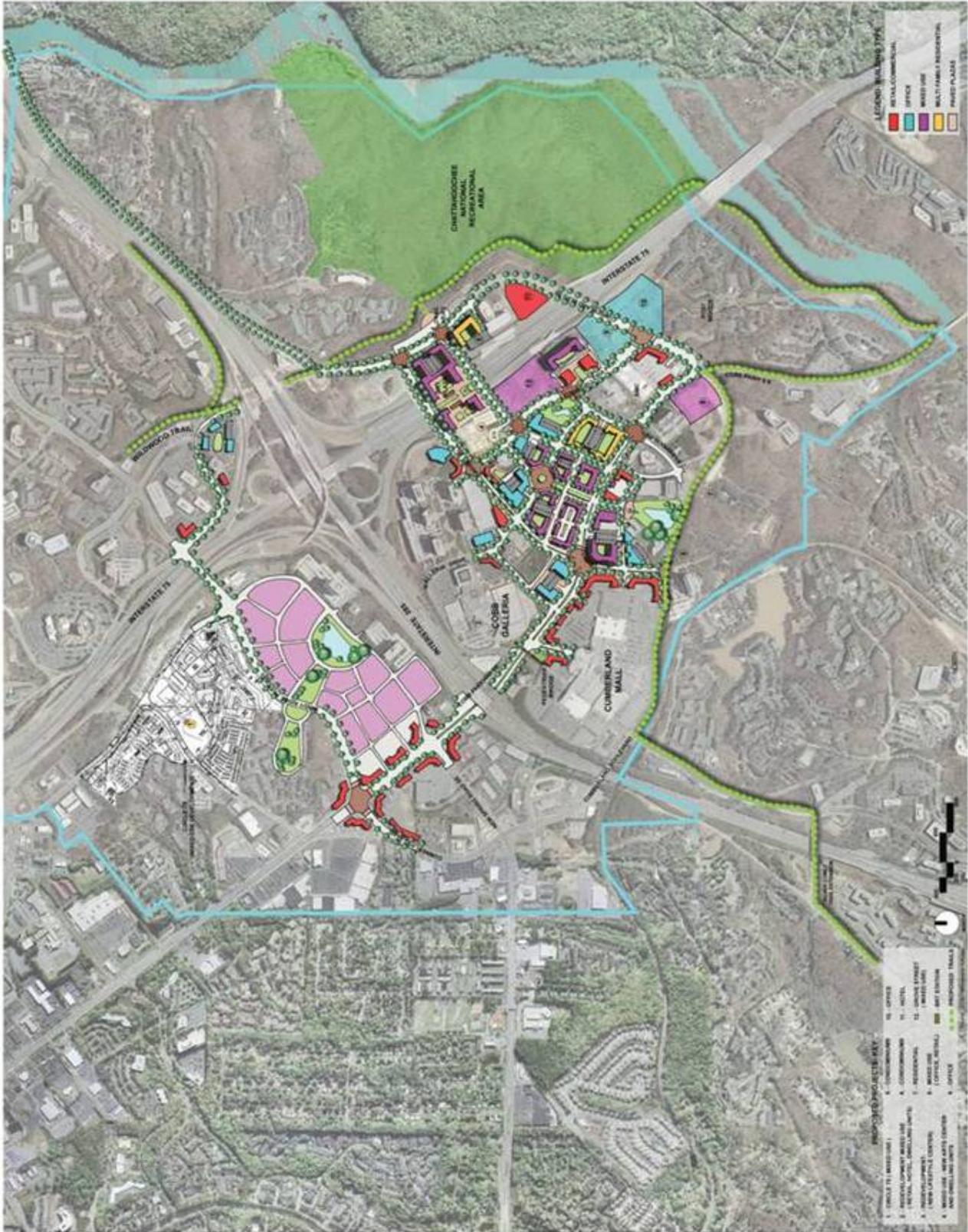
THE MASTER PLAN IS DEVELOPED AROUND THE AKERS MILL SQUARE AS THE CENTRAL CORE, WHICH IS NOT UP FOR REDEVELOPMENT IN THE NEAR FUTURE

THE MASTER PLAN DOES NOT PROVIDE RECOMMENDATIONS FOR THE LARGER IMPACT AREA

THE MASTER PLAN TREATS CUMBERLAND BOULEVARD AS AN EDGE, AND NOT AS A SPINE.

IT DOES NOT ENCOMPASS DETAILED STREETSCAPING AND DEVELOPMENT ALONG CUMBERLAND BOULEVARD.

- Mixed-use development on Akers Mill Site around a small paved central plaza.
- Intense TOD at the proposed transit station with a small plaza that covers over I-75
- Linear buildings along the street on AMC Theaters parking lot



## Option B – Salient Features

## Additional Features that differ from other Master Plan options:

### STAKEHOLDER COMMENTS

#### PROS:

CONNECTION SOUTH OF  
transit STATION FROM  
CUMBERLAND BOULEVARD  
TO COBB PARKWAY

CENRTAL GREEN SPACE

RETAIL ON THE CORNER OF  
COBB PARKWAY AND AKERS  
MILL ON THE ARTS CENTER  
SITE

TOD AROUND BRT STATION  
AND OTHER ADJACENT  
DEVELOPEMNTS

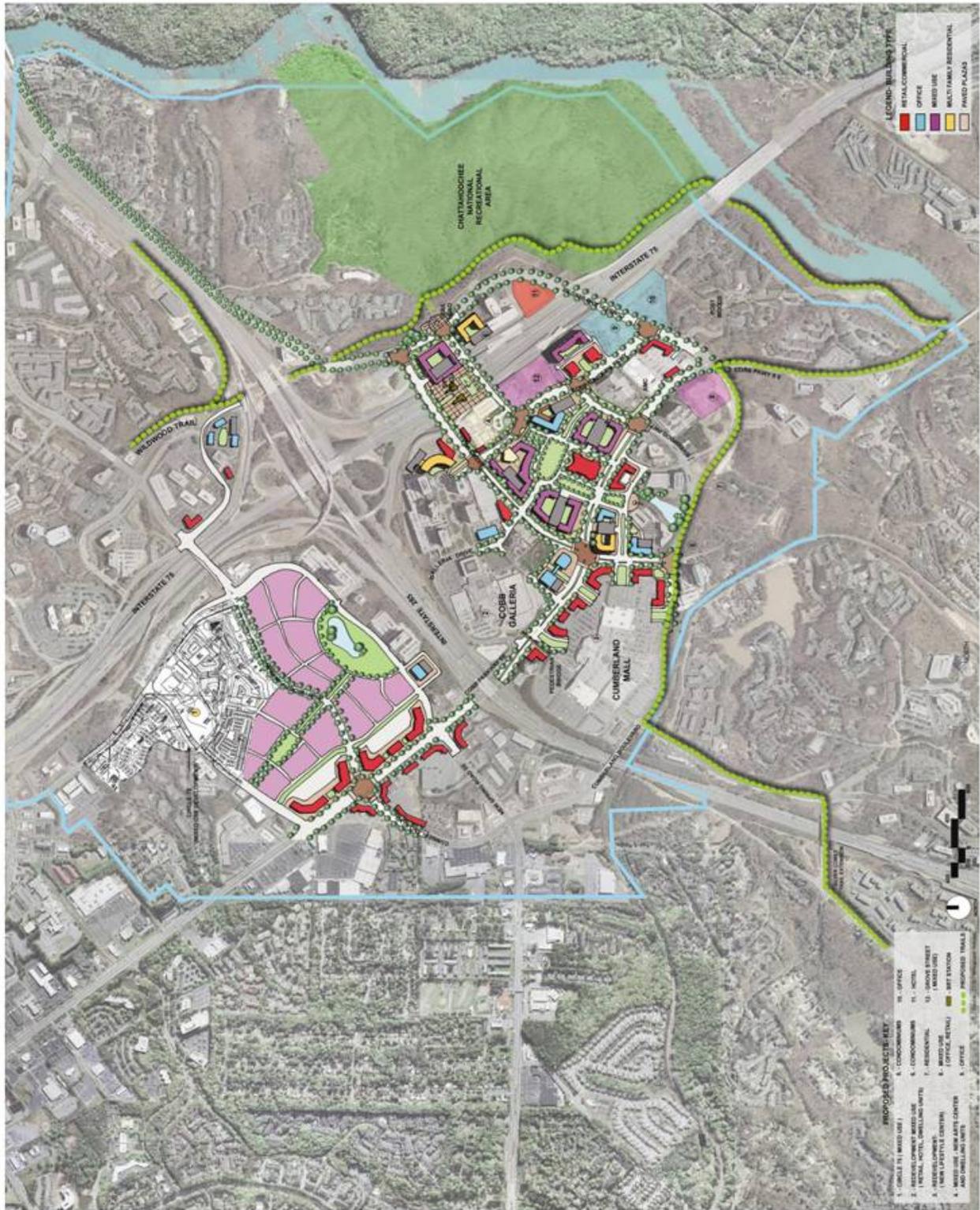
#### CONS:

PROVIDE  
RECOMMENDATIONS FOR  
transition SOUTH OF  
CUMBERLAND BOULEVARD

THE TOD AT THE BRT STATION  
IS LESS INTENSE.  
PROVIDE MAXIMUM  
DENSITIES AT transit STATION  
AND MID RISE DEVELOPMENT  
ON AKERS MILL SITE

THE PLAN INDICATES MIXED  
USE ALONG RIVERWOOD  
PARKWAY. PROVIDE FOR  
MORE OFFICE USES.

- Greenspace and pocket parks within the mixed-use district
- Large village green/public/civic space to be the focal point for the entire district
- Transit station with a large public plaza that covers over I-75 with a less intense TOD adjacent to it
- Linear retail building to the Performing Art Centre to strengthen the Akers Mill street side activity



**BLUEPRINT CUMBERLAND PHASE II**  
**MASTER PLAN OPTION - B**



## Option C – Salient Features

## Additional Features that differ from other Master Plan options:

### STAKEHOLDER COMMENTS

#### PROS:

THE CENTRAL LINEAR GREEN SPACE IS A GREAT IDEA AND PROVIDES A USABLE PUBLIC SPACE IN THE AREA. IT ALSO STRENGTHENS THE CONNECTION TO THE ARTS CENTER AND THE NATIONAL PRESERVE

THE FINER-GRAINED STREET NETWORK PROVIDES MULTIPLE ROUTES TO A DESTINATION AND GIVES THE AREA A MORE URBAN FEEL.

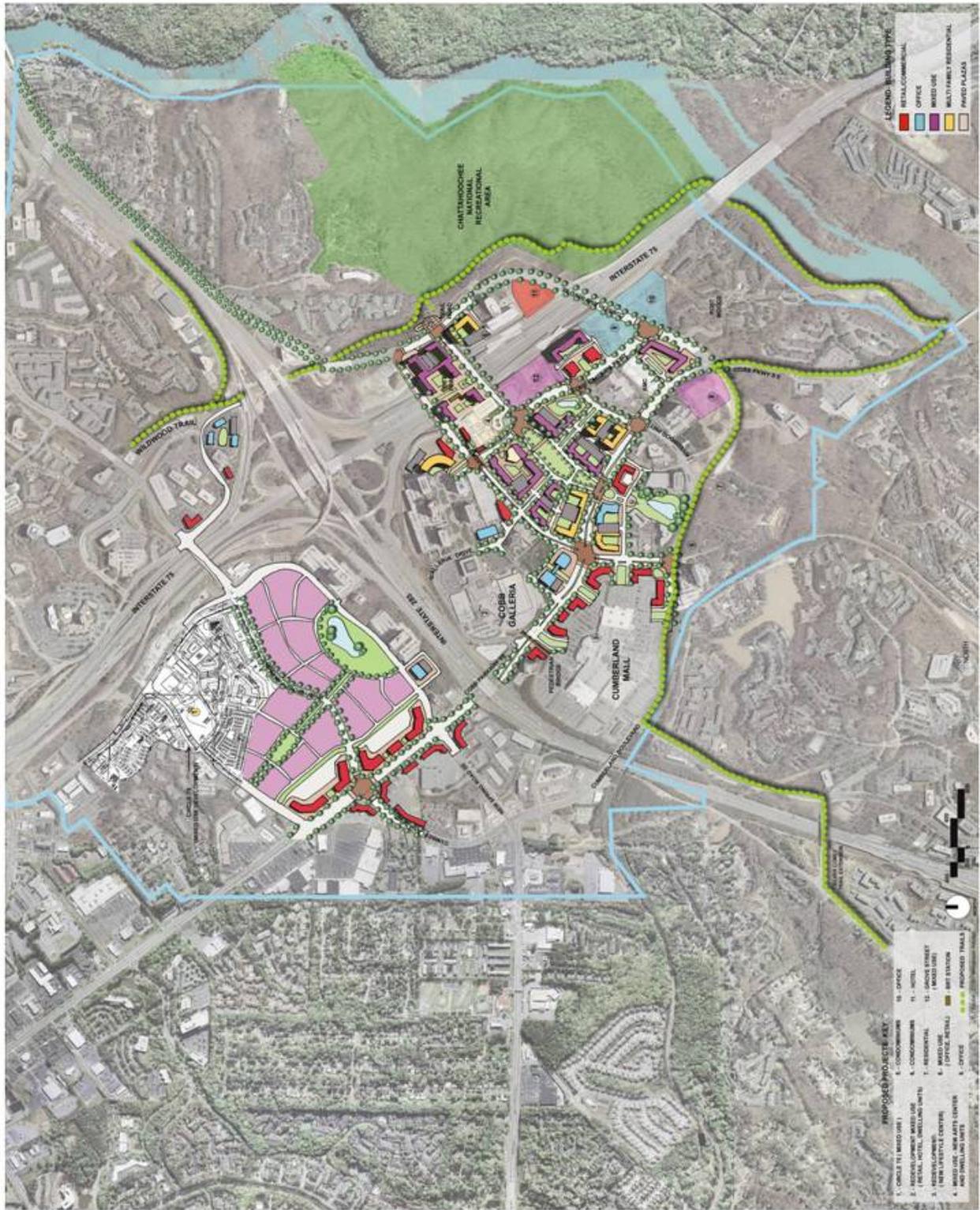
INTENSE TOD WITH A PUBLIC PLAZA AROUND THE transit STATION IS AN APPROPRIATE PLACE FOR HIGHER DENSITY USES AND HIGH RISE BUILDINGS.

#### CONS:

CONNECT COLUMNS DRIVE BIKE FACILITIES TO THE WILDWOOD TRAIL AND PERIMETER CID AREA.

PROVIDE FOR INTER PARCEL CONNECTIVITY IN PRIVATE DEVELOPMENT. THIS WILL KEEP LOCAL TRAFFIC OFF OF MAIN CORRIDORS.

- Greenspace and pocket parks within the mixed-use district
- A large village green public/civic space is the focal point for the entire district. This greenspace connects Cobb Parkway to the Chattahoochee National Recreation Area.
- Intense TOD around transit station with a large public plaza that covers over I-75
- Linear retail building to the Performing Arts Centre to strengthen the Akers Mill street side activity
- Mixed-use development at the AMC plaza block
- Linear buildings with view corridor along the periphery of Cumberland Mall



## RECOMMENDED MASTER PLAN

Based on the input and comments from the workshop on the three concept plan, a final Concept Plan was developed.

The vision behind Blueprint Cumberland II is to transform the Cumberland/Galleria area from office/retail that generates only daytime activity to a 24/7 lifestyle activity center that supports a live-work-play environment. To create a downtown feel for the Cumberland/Galleria area with an array of mixed-uses: retail, office, entertainment, apartments, condos, townhomes, detached and attached single-family, the Master Plan lays out direction and means to achieve this goal. Any plan or vision of this nature is not expected to be realized overnight or by a single entity. It is a long-term collaborative effort that involves the County, CCID, the property owners, development community, other agencies and the community. While these recommendations are expected to be realized in the long-term where the future conditions may change, the concept plan serves as a blueprint and a guideline for development.

The Cumberland/Galleria Area would benefit from a master plan that promotes a distinctive architectural character and a vibrant discernable activity center - a *'focal point.'* The Concept Plan creates a new focal point in the Core area with a linear town green/public plaza and space for public passive and active recreation, together with an array of mixed-uses: retail, office, entertainment, apartments, condos and townhomes surrounding it. This new activity center will create a sense of place and character for the community. This linear activity center will also connect the existing activity nodes and centers within the area: Cobb Energy Performing Arts Centre, Chattahoochee National Recreation Area, Cumberland Mall and the Cobb Galleria. Apart from these prime anchors, the concept plan also integrates within itself the proposed development projects and infrastructure initiatives.

To promote a more walkable environment in the Core Area, the Master Plan proposes additional vehicular and pedestrian connections within the area. A finer-grained network of pedestrian-friendly streets, with wide sidewalks, signage and street trees, would provide multiple routes to various destinations in the Cumberland/Galleria area along with enhancing the urban environment. Active retail uses along existing and proposed streets like restaurants, cafes, shops and boutiques will further transform the streets into vibrant public spaces with a flurry of pedestrian activity.

These pedestrian-oriented streets will also transform the currently existing superblocks into smaller walkable blocks/ development pods. The development pods will be punctuated with a series on interconnected greenspaces and pocket parks to provide for relief and passive recreational spaces in a dense urban environment. Smaller blocks will also allow for the flexibility of density and uses that can be developed incrementally in phases.

For easy accessibility, the Cumberland/Galleria area also needs to offer other transportation choices to the office goers, residents and the visitors living/traveling in the area. The Master Plan proposes a public plaza integrated with the transit station that spans over I-75 and connects the Cumberland/Galleria core to the Chattahoochee National Recreation Area. The area around the transit station is proposed to be developed into a Transit-Oriented Development (TOD) with higher density and diverse uses and activities. The Master Plan also recommends other alternate means of transportation like multi-path trails and bike paths to further enhance the accessibility/connectivity to and within the Cumberland/Galleria area. Along with incorporating proposed traffic initiatives, including traffic signals, new streets and street alignments, median cuts and improved crosswalks, the plan also identifies several traffic calming measures, such as landscaped medians and paved intersections along the corridor to provide a safe pedestrian environment.

The area north of I-285 and Windy Ridge Parkway, as shown in the Master Plan, consolidates retail along Cobb Parkway with buildings lining Cobb Parkway and parking located in the rear. Pedestrian-friendly streetscapes along Cobb Parkway will enhance this corridor both visually and functionally. The remaining area is proposed to be mixed-use that is predominantly residential supported by neighborhood-based retail. The residential uses would include various product types: townhomes, single-family, condos, apartments and live/work units. The mixed-use will include pocket parks and open spaces that serve the district.

### **Land Uses and Development Density**

The Master Plan identifies various land uses within the Core Area, the extended area and the potential densities within. Much of the area as shown in the Master Plan is designated mixed-use that allows for a diversity of uses & activities, a walkable street grid, pedestrian-friendly environments, pocket parks and public spaces. This provides the required flexibility for the development to occur over a period of time as the conditions change. These mixed-use districts will provide for diverse uses: retail, restaurants, commercial, office, condos, apartments, live/work units, entertainment/recreation, hotel/conference, parks and open spaces. The mixed-use districts are further delineated into sub-areas with varying degrees of density that are based on their location, accessibility, links to activities and the proposed infrastructure initiatives. These sub-areas are designated as MXD-1, MXD-2 and MXD-3.

**MXD-1** is the area immediately adjacent to I-75 both to the east and west of I-75. With the proposed transit station on the I-75 corridor, it provides opportunity for a high density Transit-Oriented Development (TOD) that could have offices/residential in conjunction with the transit station. A public plaza is recommended to span across I-75 with towers (predominantly residential, some office and potentially a hotel included) that would integrate into the TOD. This will allow for people to live around the transit station

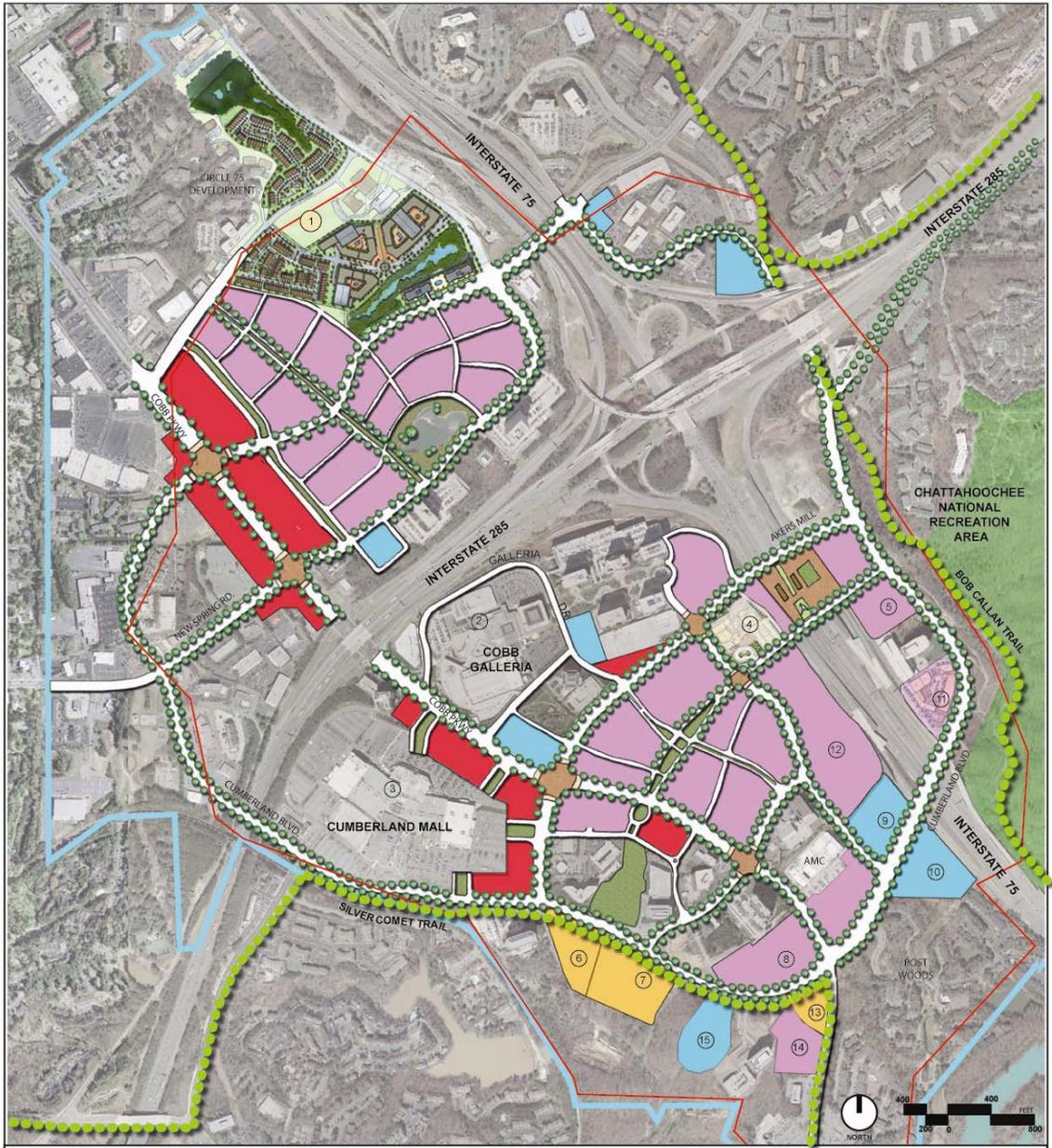
with commuting accessibility within walking distance. The high-density development also provides opportunity for more people to have convenient accessibility to the Chattahoochee National Recreation Area. Being on higher ground, the TOD provides great views of the Chattahoochee National Recreation Area. The other blocks in this district provide the same level of density and increased diversity of uses including retail/restaurants/entertainment activities. All of these blocks provide accessibility to transit within walking distance. The presence of the Cobb Energy Performing Arts Centre, within this district, adds to the diversity of entertainment and public activities. The Cobb Energy Performing Arts Centre should be an integral part of any development that is undertaken within this district. Also, a street is proposed to the south of the Cobb Energy Performing Arts Centre along with the proposed transit plaza that provides connectivity to the Chattahoochee National Recreation Area. This connectivity is crucial for linking the heart of Cumberland CID area with the unique natural resource – the Chattahoochee National Recreation Area.

**MXD-2** is the area within the Core Area of the study area, includes Akers Mill Plaza and the block south of that, the area surrounded by Cobb Parkway to the west, Akers Mill Road to the north, Cobb-Galleria Parkway to the east and Cumberland Boulevard to the south. This area offers opportunity for an array of uses and activities; retail, restaurants, entertainment, hotel, recreation, condos, apartments, public/civic spaces and pocket parks. Being a block away from the transit and the I-75 corridor, a little lesser density is recommended than the MXD-1. Currently this is a large superblock, which is recommended to be broken into smaller blocks and street grid with a spine - boulevard/greenspace (as shown in the Master Plan) - that connects Cobb Parkway to transit and the Cobb Energy Performing Arts Centre area. The smaller blocks will offer flexibility for different uses and needs. These smaller blocks will support a variety of development projects that can accommodate parking decks, buildings and pocket parks within the blocks that support, hotel, retail, office and residential activities. The minimum building

heights envisioned here are at least four stories high to provide the urban feel and pedestrian-friendly street environments. The recommended linear greenspace will provide for both active and passive recreation and public functions and activities. Plaza areas, artwork, signage, and street life along this linear green will create the heart and focal point for the Cumberland/Galleria area. This will provide the 24/7 life that will enhance the livability of the area and thus a much needed thriving “Cumberland Downtown”.

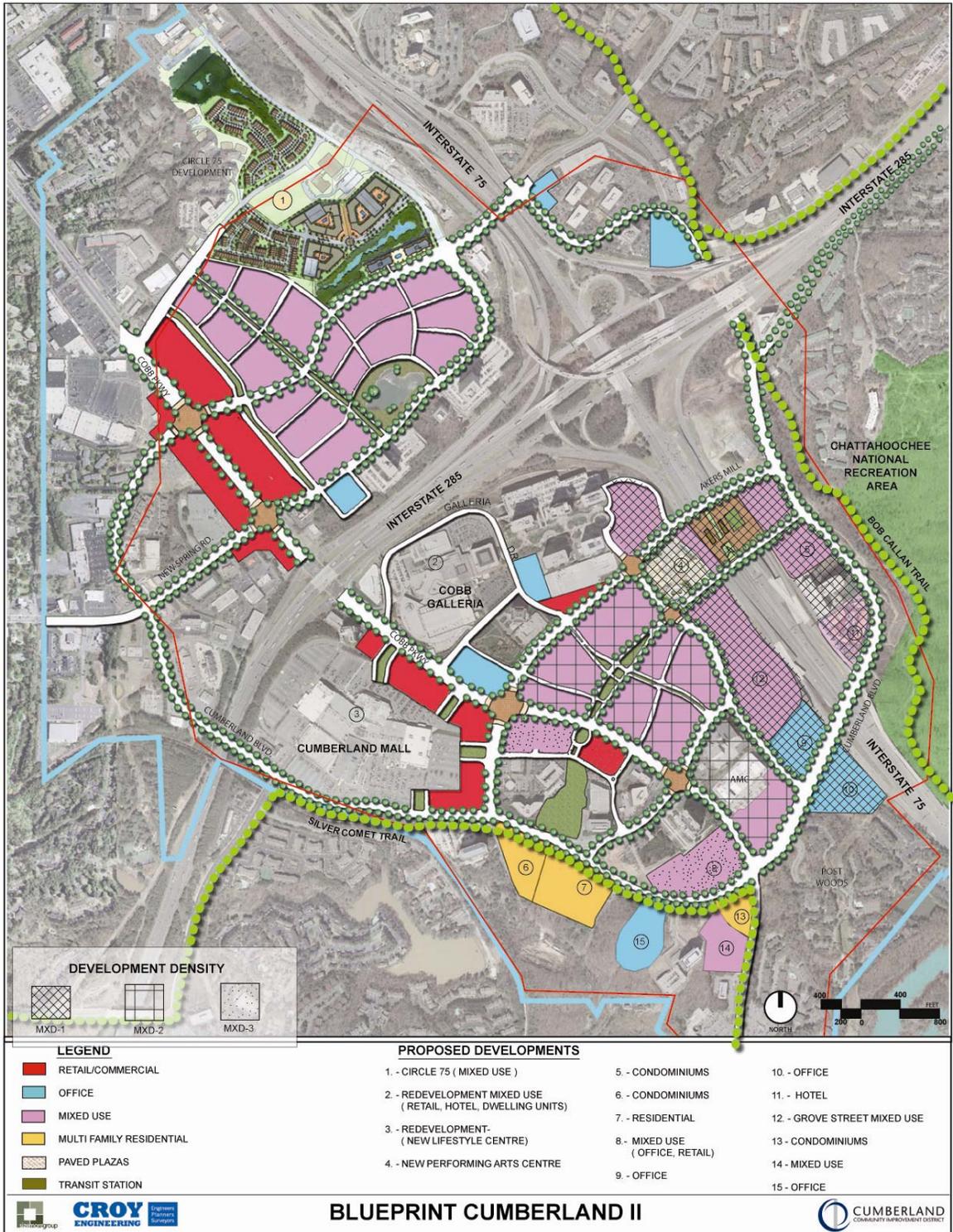
**MXD-3** is the area west of MXD-2 the area surrounded by Cobb Parkway to the east, Cumberland Boulevard to the west/south and Akers Mill to the north. There are multi-family residential projects proposed to the west of this area, between the neighborhoods and the CCID area. The topographical change between the neighborhood and the adjacent area in itself provides a necessary transition. This MXD-3 area has potential for increasing the residential and retail components along with office uses. The density within this area is recommended to be lesser than that of MXD-2. This area is proposed to add additional street network to improve the pedestrian-friendly environment. The grade difference within the area poses challenges to create a smooth, walkable environment. However, the street grid, sidewalk structure and smaller blocks will offer a pedestrian-friendly environment. The minimum building heights envisioned for this area are at least four stories high to support the walkable environment. Pocket parks are recommended to support the public and civic functions within each quadrant. Pedestrian and green linkages are recommended to link the Cumberland Mall, the proposed linear green in MXD-2 and the existing lake, multi-path trail and pedestrian infrastructure. This will provide the necessary link to the multi-path trail that connects to the Silver Comet Trail and the linkage to the Chattahoochee National Recreation Area.

Moving away from the Core Area, the land uses become less intense and predominantly residential. The Master Plan proposes a less intense residentially-focused, mixed-use district along Windy Ridge Parkway, which is in line with the existing and proposed developments. As shown in the concept plan, this area can support mixed-use that can tie into the proposed mixed-use Circle 75 development. The district will consist of consolidated retail development along Cobb Parkway and medium-density residential development (townhomes and single-family homes) organized around a central greenspace.

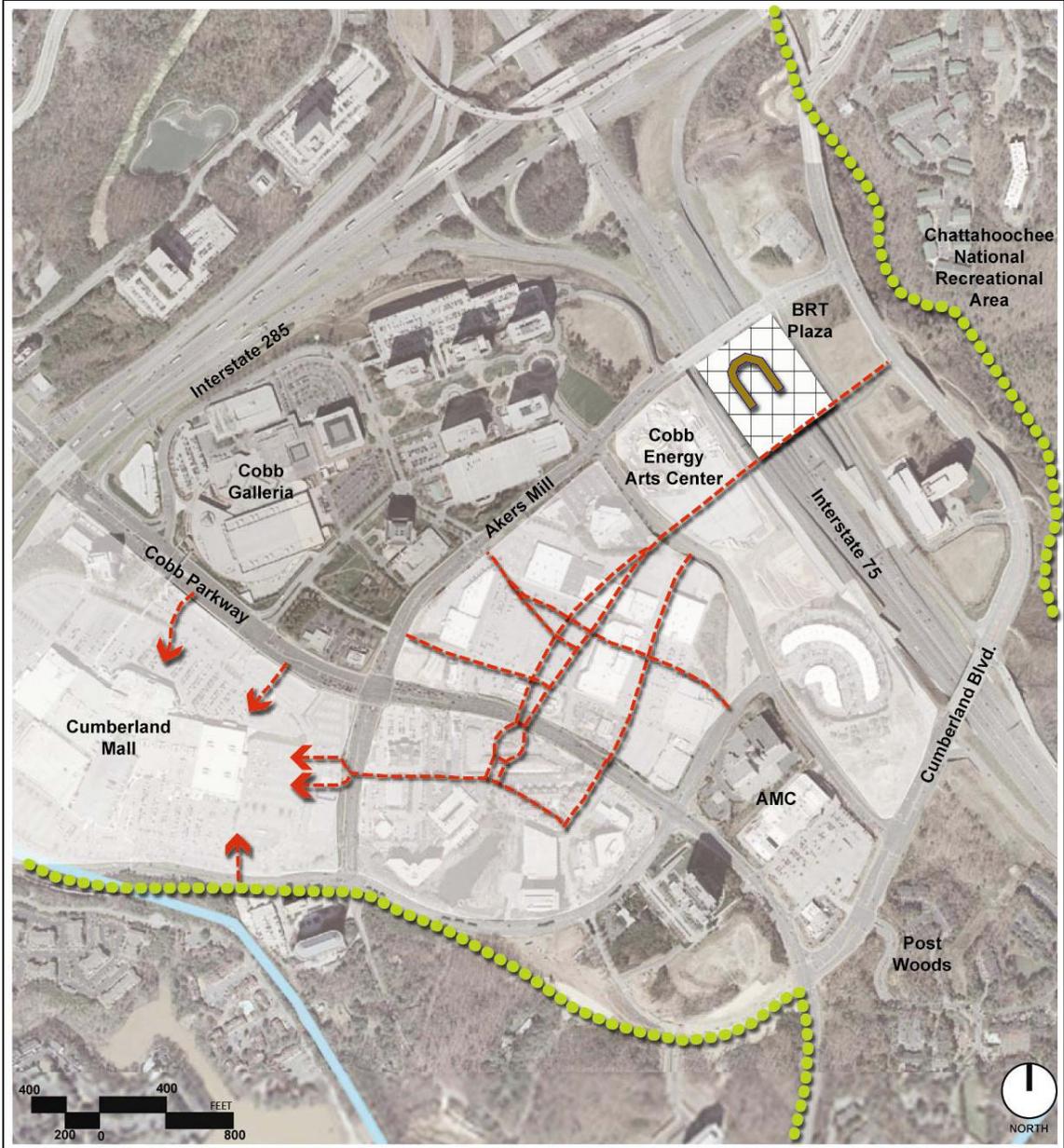


LEGEND	PROPOSED DEVELOPMENTS		
<span style="color: red;">■</span> RETAIL/COMMERCIAL	1. - CIRCLE 75 ( MIXED USE )	5. - CONDOMINIUMS	10. - OFFICE
<span style="color: lightblue;">■</span> OFFICE	2. - REDEVELOPMENT MIXED USE ( RETAIL, HOTEL, DWELLING UNITS )	6. - CONDOMINIUMS	11. - HOTEL
<span style="color: pink;">■</span> MIXED USE	3. - REDEVELOPMENT- ( NEW LIFESTYLE CENTRE )	7. - RESIDENTIAL	12. - GROVE STREET MIXED USE
<span style="color: yellow;">■</span> MULTI FAMILY RESIDENTIAL	4. - NEW PERFORMING ARTS CENTRE	8. - MIXED USE ( OFFICE, RETAIL )	13. - CONDOMINIUMS
<span style="border: 1px solid black; display: inline-block; width: 10px; height: 10px;"></span> PAVED PLAZAS		9. - OFFICE	14. - MIXED USE
<span style="color: green;">■</span> TRANSIT STATION			15. - OFFICE

Master Plan

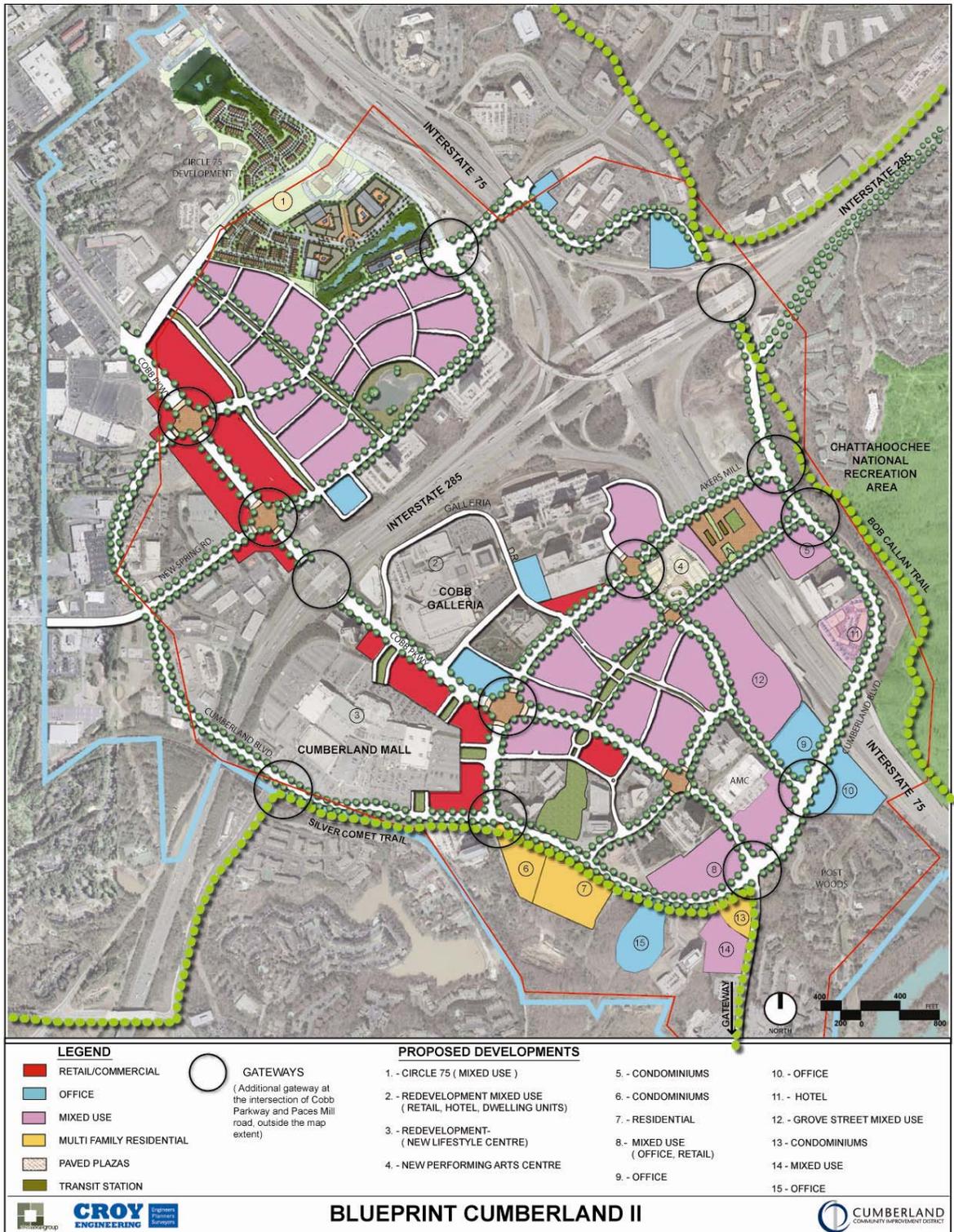


Master Plan - Densities



New Street Connections	<b>Street Network</b>
	<b>BLUEPRINT CUMBERLAND II</b>

Master Plan Street Network



**Master Plan – Gateways**



<ul style="list-style-type: none"> <li> Existing Streets with enhanced streetscapes</li> <li> Proposed Pedestrian oriented Streets</li> <li> Paved crosswalks on major intersections</li> </ul>	<ul style="list-style-type: none"> <li> 1/4 Mile Radius 5 minute walking distance</li> <li> 1/2 Mile Radius 10 minute walking distance</li> </ul>	<h3>Pedestrian Connectivity and Walking Distances</h3>
	<h3>BLUEPRINT CUMBERLAND II</h3>	

Master Plan – Walking Radius



<ul style="list-style-type: none"> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: #90EE90; border: 1px solid black; margin-right: 5px;"></span> Green Spaces</li> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: #FFD700; border: 1px solid black; margin-right: 5px;"></span> Development Pods</li> <li><span style="display: inline-block; width: 10px; height: 10px; background-color: #90EE90; border: 1px solid black; border-radius: 50%; margin-right: 5px;"></span> Trails</li> </ul>	<h3>Green Space Connectivity</h3>	
	<h2>BLUEPRINT CUMBERLAND II</h2>	

**Master Plan – Green Space Connectivity and Development Pods**



Master Plan – Aerial View



Master Plan – Street View

## **REVIEW OF PLANNING TOOLS BY PEER COMMUNITIES AND BUSINESS DISTRICTS**

### **INTRODUCTION**

As a part of the Blueprint Cumberland II planning process, a number of “cutting-edge” planning tools were reviewed. One of the planning tools that has been readily accepted by peer communities and business districts throughout the Southeast and elsewhere in this country is called “form-based coding.” Peer communities with form-based codes include Arlington, VA; Farmers Branch, TX; Ft. Myers, FL.; St. Lucie County, FL.; Cape Coral, FL; Contra Costa County, CA; Petaluma, CA; Woodford County, KY; Sarasota County, FL; Onondaga County, NY; Baltimore County, MD; and Blue Springs, MO to name just a few.

Currently, Cobb County and its municipalities use traditional zoning codes that divide parcels into different zones/districts (residential, commercial, industrial, etc.); list those allowable land uses in each zoning district; and, indicate the density allowed by regulating such items as building heights, floor area ratios, parcel coverage, setbacks, and other dimensions. By comparison, form-based codes regulate the streetscape, form, and the public realm. In general, these alternatives govern building types, building dimensions, parking locations, and façade features.

Form-based codes are prescriptive, because they clearly state those building types, streets and civic spaces down to the parcel level, that are desired. In this way, form-based codes make it easier for decision-makers to deal with the details of mixed-use, transportation alternatives and other factors.

Form-based codes allow decision-makers to connect building types and street types at a micro level. For example, some small- and medium-sized homes are planned on a side street adjacent to an urban arterial with a poor Level of Service. An “endcap” building, not allowable under traditional zoning, can block those houses from the noise of the busy arterial and provide privacy. Under a form-based code, the end building could include senior and workforce housing, offices for local businesses and retail stores, not found in single-family zoning. These buildings, under a form-based code, are designed to match the scale of roads and the surrounding neighborhood.

### **OVERLAY DISTRICTS**

Another popular planning tool for accommodating special situations, like those in the CCID and Cobb County, is the overlay district. This is a special zone, created by ordinance, which has its own unique, additional standards (or a schedule of items that automatically replace the requirements in the conventional zoning ordinance). One reason the overlay district is popular is that the county can “overlay” a set of standards for the area without a complete rezoning of the county, while maintaining the status quo of zoning elsewhere in the county.

The overlay district tool is applied in several ways. The more traditional approach is for the overlay district standards to be optional. The overlay standards are offered to the developer as alternatives to the conventional standards. Incentives might be provided to the developer to opt for the alternative standards (such as shallower setbacks which increase buildable area).

Currently, Cobb County has an overlay district that has been applied to several areas throughout Cobb County. The full text of this overlay district, which could be amended into a form-based code overlay and applied to the Cumberland CID area, is presented on the following pages.

## **“Sec. 134-221.2. Redevelopment overlay district (ROD)**

The regulations for the ROD are as follows:

1. Purpose and intent. The ROD is established to provide locations for redevelopment of commercial, office and residential uses which are pedestrian oriented and developed at a community or regional activity center scale and intensity, as identified for each specific site or corridor via the Cobb County comprehensive plan, as may be amended from time to time. This is intended to encourage compatible mixed-uses within the boundaries of these properties. The district may be overlaid upon the LRO, LRC, NRC, O&I, CRC, RMR, OMR, RHR, OHR, NS, PSC, TS and GC zoning districts within these redevelopment corridors and specific redevelopment sites. The district may also be overlaid upon the RM-12 and RM-16 districts that are adjacent to commercially zoned properties within these redevelopment corridors and specific redevelopment sites. The board of commissioners has determined that any redevelopment project approved within a ROD shall not establish any type of precedent for land use recommendations or future rezoning proposals outside of the boundaries of the ROD project. Those properties outside of the boundaries of a ROD project must provide uses compatible with other surrounding properties that are outside of the ROD. This ROD may be applied to properties within the corporate limits of Marietta and Smyrna, at the discretion of the respective city councils.
2. Transit/land use guidelines.
  - a. Transit stations are not merely bus stops. A transit station would have considerable parking for vehicles, and perhaps other mechanisms for gathering riders at the location, other than just pedestrians in the immediate area. A transit station would be a major collector point for collecting and distributing riders primarily for regional commutes, but would also serve as a connecting point for local commutes. It would have a building, as well as a large amount of parking, and might also be served by satellite parking and shuttles, circulators, and local bus routes. Transit stations would support intense residential and commercial land uses adjacent to the transit station. Transit stations would be unsuitable in locations where the result would be an increase in vehicular traffic through existing neighborhoods, or where it would intensify land uses within existing stable low-density neighborhoods. Transit stations should be located adjacent to, or very close to interstate highways, and only in locations where it will not increase vehicular traffic within nearby low-density neighborhoods, and where it will not intensify land uses within existing low-density neighborhoods.

b. Transit exchanges are significantly less intense than transit stations, but they are more than just bus stops. Transit exchanges may provide limited parking for vehicles. Transit exchanges would be collector points, and provide opportunities for interconnection and transfer of various transit routes. When transit exchanges are located in relatively intense commercial or mixed-use areas, transit exchanges can support moderate intensification of land uses. Transit exchanges should not be located in low-density residential areas, but may be located within high-density residential areas. When transit exchanges are located near areas that are primarily single-family homes, land uses near the transit exchange should be of a limited "neighborhood-intensity". In areas characterized by high-density residential, more intense future land uses can be supported. Transit exchanges should be located only along major arteries (at least four lanes), and only in locations where it will not increase vehicular traffic within nearby neighborhoods, and where it will not intensify land uses within existing neighborhoods. A transit exchange may share a tract of land with another land use or building and its parking. A transit exchange may utilize shared parking facilities.

c. Transit stops are a designated place where a local transit vehicle would stop for passengers from the immediate area. There would be no parking in the vicinity designated for transit riders. There should be no increase in the intensity of land uses near a transit stop. Land uses would not be changed or intensified based on the existence of a transit stop.

d. Satellite parking facility would be similar in some ways to a transit exchange. Satellite parking facilities should only be located near major corridors, and if possible should have multiple access points. They should be designed for efficient ingress and egress. They could be located in areas near low-density single-family homes, but if located in such areas, adjacent land uses would have to be limited to low-or-medium density residential, and limited neighborhood-compatible retail. Land uses on all properties not adjacent to the satellite parking facility would remain consistent with any low-density single-family land uses in the area. Satellite parking would be served by shuttles that would take commuters to nearby employment centers and transit stations. In some cases satellite parking facilities could be served by transit that would proceed from satellite parking facilities and go directly into an established transit route. A satellite parking facility can facilitate transit serving more than one transit route (destination). The difference between a satellite parking facility and a transit exchange is that a transit exchange has limited parking and facilitates transfers between transit vehicles, while a satellite parking facility facilitates community parking and an access point to access transit to get to other destinations. A satellite parking facility may share a tract of land with another land use or building and its parking. A satellite parking facility may utilize shared parking facilities.

3. Permitted uses. Permitted uses are as follows:
- Athletic and health clubs.
  - Automotive parking lots or garages.
  - Banks and financial institutions with automated transfer machines; however, no drive-in establishments are permitted.
  - Clinics, clubs or lodges.
  - Commercial indoor recreation uses.
  - Community fairs.
  - Commercial retail uses.
  - Condominiums.
  - Convenience food stores with self service fuel sales, provided that the building shall not exceed 3,000 square feet in gross floor area and that no automotive repairs shall be done on site.
  - Corporate or administrative office for any permitted uses.
  - Cultural facilities.
  - Designated recycling collection locations.
  - Eating and drinking establishments.
  - Film developing and printing facilities.
  - Full service gasoline stations.
  - Group homes.
  - Hotels.
  - In-home day care.
  - Laundry and dry cleaning pickup establishments.
  - Medical and dental laboratories provided no chemicals are manufactured on-site.
  - Multi-family dwelling units.
  - Neighborhood retail uses.
  - Non-automotive repair service establishments.
  - Nursery schools and child day care centers.
  - Office service and supply establishments.
  - Parking for vehicles.
  - Photograph studios.
  - Printing, publishing and lithography establishments.
  - Private parks.
  - Professional offices.
  - Radio, television and other commercial towers and antennas, subject to section 134-273.
  - Rest homes, personal care homes and convalescent homes.
  - Single-family dwelling units (attached and detached).
  - Studios and supplies.
  - Self service laundry facilities.

4. Lot size and setback requirements. See use limitations.
5. Landscape buffer and screening requirements. Unless otherwise noted within this district's requirements, any property within an ROD which abuts residentially zoned property shall have a minimum 25-foot landscaped screening buffer adjacent to all residentially zoned property, which will be subject to county staff approval. Required buffers may be included within required setbacks; however, in such case that the required buffer is greater than the required setback, the required buffer shall be adhered to. Additionally, necessary private utilities and access drives may be allowed through, over or across a landscaped buffer. Any such uses which are proposed through, over or across a designated undisturbed buffer must be approved pursuant to an original site plan or site plan modification as set forth under section 134-126.
  - a. Objectives. Maintained, natural buffers and berms shall be implemented in connection with a permitted project and shall address the following objectives:
    1. Screening to enhance aesthetic appeal;
    2. Control or direction of vehicular and pedestrian movement;
    3. Reduction of glare;
    4. Buffering of noise; and
    5. Establishment of privacy.
  - b. Standards. Buffers or berms shall be required when a ROD is located adjacent to a residential district; a minimum 25-foot buffer is required.
6. Buffers. Landscape buffers are subject to review and approval by county staff in accordance with the following standards:
  - i. Plantings are to be a mix of evergreen trees and shrubs.
  - ii. Species are to be ecologically compatible to the site and appropriate for the design situation.
  - iii. Unless public safety concerns dictate otherwise, buffers should provide a minimum visual barrier to a height of six feet within two years of planting.
  - iv. Minimum height of plant materials at installation is five feet for trees and two feet for shrubs.
  - v. Fencing or walls are to be a minimum of six feet in height as approved by county staff.
  - vi. Trees included in buffer plantings may be counted toward site density calculations as required by chapter 50, article VI, pertaining to tree preservation and replacement, subject to review and approval of county staff.

- vii. Buffers shall be regularly maintained by the property owner to ensure that the objectives and standards set out in this subsection are met.
- viii. When topography and existing conditions allow, the required 25-foot buffer should be a maintained, natural buffer.
- ix. Any appeals from a determination by county staff shall be to the board of zoning appeals.

- 7. Berms. Berms are subject to review and approval by county staff in accordance with the following standards:
  - i. Berms shall be utilized when consistent with surrounding property features.
  - ii. Berms shall be stabilized.
  - iii. Where possible, berms shall be constructed to be consistent with natural or proposed drainage patterns.
  - iv. Berms shall be regularly maintained by the property owner.
- 8. Floodplain and wetlands preservation requirements. Any development must meet state and federal requirements relating to areas subject to the provisions of section 134-283, regarding mountain and river corridor protection act areas, and section 134-284, regarding metropolitan river protection act areas. No floodplains and/or wetlands may be used in calculating the overall density of the development.
- 9. Building and structure requirements. See use limitations.
- 10. Parking requirements. See section 134-272 for paved parking specifications. Parking for non-residential or multifamily uses may be granted a 20 percent reduction in required parking when parking is shared between adjacent uses within the project. An additional ten percent reduction may be administratively approved by the director of community development, or his/her designee. Final parking design plans shall be subject to review and approval of the director of community development, or his/her designee.
- 11. Lighting requirements. Any project permitted within the ROD district which proposes a lighted facility must have a county department of transportation approved lighting plan in accordance with the minimum conditions listed in section 134-269.

12. Procedures for ROD overlay utilization. As the ROD is overlaid upon an existing zoning district, the project will be reviewed and approved or denied in a streamlined manner. Staff will accept applications, then review and recommend approval or denial. A schedule of application submittal deadlines, concept plan review meetings, and projected planning commission and board of commissioners zoning dates will be made available to the public. Even though the underlying zoning will not change, staff recommendations will be taken to the planning commission and board of commissioners as a regular zoning item on the next available zoning agenda.

ROD proposals are required to be posted for 30 days prior to the planning commission and board of commissioners meeting. In addition, public hearings will be held at the time the planning commission and board reviews and decides each proposal. If the project is denied by the board of commissioners, no prejudice period will apply. Further, upon gaining approval of an ROD overlay plan, the applicant maintains the option to develop the property according to the requirements of the underlying zoning.

The following procedure will apply:

- a. Application. Applications for ROD overlay district utilization with an existing zoning will be accepted in the planning division of the county community development agency. The application fee is \$100.00.
- b. Concept plan review. There will be a regularly scheduled ROD review meeting of the county staff scheduled twice monthly. Conceptual approval must be obtained prior to placing the property on an agenda.

13. Purposes.

- i. Familiarize sponsors of projects with county regulations and the concerns of county agencies prior to expenditure for preparation of final development plans.
- ii. Familiarize agency representatives with proposed project and provide an opportunity for an exchange of views and ideas on project characteristics that are of concern to the goals of the ROD ordinance.

14. Participants. The county staff to be present include one representative from the following agencies and divisions:

- i. Cobb County community development, planning division and zoning division.
- ii. Cobb County water system.
- iii. Stormwater management.
- iv. Cobb County department of transportation.
- v. Site plan review/county arborist.
- vi. Cobb County fire marshal's office.

15. Initiation of concept plan review. Concept plan review shall be initiated by the filing of the following items with the appointed representative of the planning division:

- i. A completed application form stating that the applicant is the property owner or authorized agent.
- ii. A letter of intent that specifies the types of uses desired within the redevelopment proposal.

16. The following plans and materials shall be submitted to the planning division both in electronic and paper form:

- i. A current plot plan and boundary survey showing:
  - (a) The architect, engineer, or designer's name, address, and telephone number,
  - (b) Scale of plan and north arrow,
  - (c) Street address of site and vicinity map showing the relationship of the site to the surrounding area,
  - (d) Existing land lot, property lines, right-of-ways, dedications, and easements,
  - (e) Locations of existing and proposed structures, driveways, walks,
  - (f) Delineation of floodplain and wetland areas,
  - (g) Locations of any known cemeteries or historic sites,
  - (h) Conceptual architectural elevations,
  - (i) All ROD projects shall be governed by an approved Concept Plan. Upon approval of the Concept Plan, individual pods of the redevelopment project may be undertaken thru the county's normal plan and plat review process. ROD projects may be constructed as a single phase, or may be constructed in multiple phases, in accordance with the approved Concept Plan.

17. Use limitations.

- a. In order to encourage pedestrian oriented mixed-use development, traditional lot by lot restrictions such as minimum lot sizes and setbacks shall not apply. Rather, all projects must be consistent with the concept plan, as approved by the board of commissioners.
- b. Minimum acreage of five acres. Smaller tracts may be considered appropriate if within 200 feet of existing or proposed redevelopment project, within the designated redevelopment corridors.
- c. Design of entire project must be consistent with section 427 of the Cobb County development standards (urban design standards) as may be amended from time to time.
- d. Building height to be designed to provide compatibility with adjacent uses. Building orientation towards the public street with emphasis on pedestrian entrances and orientation.
- e. Development/redevelopment proposals must demonstrate a mixture of residential and non-residential land uses. At least 20 percent of the proposal's land uses must be non- residential. Higher residential densities should be located adjacent to or within close proximity to interstate highways and interchanges.
- f. Loading and service areas should be located within the interior of the project, or screened through the use of building elements, opaque walls or fences.
- g. Proposed setbacks should create a contiguous and consistent building edge along a public sidewalk (which exists or is proposed).
- h. Surface parking should be minimized by the use of a parking deck that is designed to resemble a building, or surface parking is located parallel to local streets to enhance pedestrian safety.
- i. Public plazas should be integrally connected to the proposal by pedestrian zones including porches, covered awnings, sidewalk cafes, storefront shops and street furniture.
- j. Public plazas should include a significant community gathering place such as a stage, garden, monument or educational feature.
- k. If transit service is available, transit stop should be integrally connected to the proposal by pedestrian zones including porches, covered awnings, sidewalk cafes, storefront shops and street furniture.

l. If transit service is available, transit station should be integrally connected to the proposal by pedestrian zones including porches, covered awnings, sidewalk cafes, storefront shops and street furniture.

m. In a mixed-use scenario, ten percent of the proposed residential units must be designed as "workforce" housing. For the purpose of this section, "workforce" housing shall mean units intended for occupancy (rental or ownership) by household earnings no more than 80 percent of the Atlanta Metropolitan Statistical Area's (MSA) median household income, as may be adjusted from time to time.

n. Development/redevelopment proposals must comply with the administrative standards of the Cobb County tree preservation and replacement ordinance. These standards may be reduced up to ten percent (RDF-replacement density factor) if xeriscaping is implemented.

o. Development/redevelopment proposals must include a property owner's association with bylaws or covenants containing the following minimum provisions:

17. Governance of the association by the Georgia Property Owner's Association Act (O.C.G.A. 44-3-220 et seq.) or a successor to that Act that grants lien right to the association for maintenance expenses and tax obligations.
18. Responsibility for maintenance of common areas, buffers and recreation areas.
19. Responsibility for insurance and taxes.
20. Automatic compulsory membership of all property owner and subsequent lot purchasers and their successors; and compulsory assessments.
21. Conditions and timing of transferring control of the association from the developer to the property owners.
22. Guarantee that the association will not be dissolved without advance approval of the board of commissioners.
23. Restriction of time of commercial deliveries and dumpster pickup.

p. If there is a specific corridor plan, the provisions of the ROD cannot cause less restrictive criteria to apply to the corridor plan, if the corridor plan has criteria that are more restrictive. (Ord. of 1-24-06)"

## **WHAT ARE FORM-BASED CODES?**

Prior to instituting a Form-Based Code, a critical step for the Cumberland CID is to decide what it wants to be, physically, and then derive its approach to development from that "physical vision." The development guidelines should be designed to catalyze or attract certain desirable changes, rather than permit-processing procedures. The "vision" really should be visual, not just textual. Stakeholders can collaborate with urban planners and designers to craft drawings and images of the kinds of development they feel meet this vision and then convert these images into text. Much of this work has already been accomplished during the Blueprint planning process.

Design is the thread that connects places. The streets are well-defined, well-proportioned public spaces. The streets also produce a network of parcels; residential areas are more mobile for pedestrians in grid patterns where blocks are smaller; and the architecture is preferred because it matches the area's context. Parcels and buildings have clearly defined fronts and backs. Public spaces are very desirable when placed at the end of the street or being the focal point of a plaza or square.

Design-based codes with illustrations are more prescriptive. Drawings, photos, diagrams, and sketches communicate more clearly than text what is allowed in the code. A map or "regulating plan" demonstrates the street type, the property fronts, and the district the property is in, and the category that applies to the situation. Then, the developer reads across a matrix to locate the rules for his or her district, along with any general provisions that apply to all situations.

Form-based code alternatives really put forth the old adage that a "picture is worth a thousand words."

Bill Spikowski states that there are many advantages of form-based codes such as:

- √ Because they are prescriptive, form-based codes can achieve a more predictable physical result. The elements controlled by form-based codes are those that are most important to the shaping of a high-quality built environment.
- √ Form-based codes encourage stakeholder involvement.
- √ Form-based codes encourage independent development by assembling multiple properties. This reduces the tendency to create large land assemblies and large projects that are frequently proposed for such parcels.
- √ Form-based codes result in a diversity of architecture, materials, uses, and ownership that can only come from the actions of many independent players operating within an agreed-upon vision and legal framework.
- √ A Form-based code works well in established areas, like the Cumberland CID, because it effectively defines and codifies the area's existing "DNA." Vernacular building types can be easily replicated; promoting infill that is compatible with surrounding structures.
- √ Non-professionals prefer these codes since they are much shorter, more concise, and organized for visual access and readability.

Not to be confused with advisory statements such as design guidelines or general statements of policy, form-based codes are regulatory. Ultimately, the form-based code is a tool; the quality of development outcomes is dependent on the quality and objectives of the Blueprint Cumberland II Master Plan that a code would implement.

Form-based codes are often used to align various uses on streets and blocks so they can act in unison to make an attractive "public place." This place-making may be a park or plaza, but it is most often a streetscaped thoroughfare with appropriate levels of service.

Well-written, form-based codes are more objective and easier to implement than design guidelines and they avoid most of the types of quarrels that erupt over architectural style. A form-based code focuses on results—the creation of desirable physical places as envisioned. The code is ideal for the Cumberland CID that favors a fundamental change in its urban form and character, and transforms its vision into physical reality.

To the resident, business owner or tourist, the real estate value is based on the character of the place – how the street looks and feels, how the buildings meet the street, how easy it is to walk from home to a school or a park or a restaurant. Therefore, form-based codes are geared toward the qualities of the place, and then regulate form over use to ensure compatibility.

### **Alternative Optional Form-Based Codes**

Some communities in the U.S. have adopted form-based codes as “optional codes” that can be selected to govern proposed developments. This approach can result in incrementally better development because applicants will generally face a more predictable and straightforward development review process under a form-based code. Also, development is more cost-effective and more marketable because it directly relates to the vision and character of the area in which the development is built.

In fact, some peer communities that were studied have combined both the form-based code planning tool and the overlay planning tool and crafted a “form-based overlay district.”

The base principle of form-based coding is that design is more important than use. Simple and clear graphic prescriptions for building height, how a building is placed on site, and building elements (such as location of windows, doors, etc) are used to control development. Land-use is not ignored, but regulated using broad parameters that can better respond to market economics, while also prohibiting undesirable uses. A form-based code is a land development regulatory tool that places primary emphasis on the physical form of the built environment with the end goal of producing a specific type of “place.”

The form-based code will actually streamline the development review process because it provides clear parameters, based on the Blueprint Cumberland II Master Plan. It will become easier to develop properties that follow the Master Plan and comply with the form-based code. All existing buildings and businesses can operate as they are under the new form-based code. The new regulations will only place limitations on expansion or redevelopment of non-conforming uses or buildings.

Form-based codes can be adopted under a variety of scenarios, including changing/replacing the existing zoning, creating a special district, or an optional overlay district. The Form-Based Code establishes districts that are intended to provide for the orderly transition of certain portions of the Overlay District from mostly retail and/or office areas into pedestrian-oriented districts that accommodate a mixture of retail, office, and residential uses, while protecting existing businesses. This Code implements the following goals:

- √ Protect the existing job base;
- √ Develop mixed-use cores;
- √ Provide transportation, pedestrian, and visual connectivity;
- √ Create usable outdoor areas; and,
- √ Develop safe, well-designed urban neighborhoods.

These goals are further implemented through the following objectives:

- √ Create a walkable community improvement district that encourages pedestrian mobility and reduces the dependence on the automobile.
- √ Develop a framework with the appearance, location, and scale of buildings that complement the character of the area.
- √ Ensure compatibility between existing and proposed businesses.
- √ Provide a mix of building types.
- √ Provide a variety of passive and active greenspace.
- √ Establish sustainable new urban development.

### **Legal Basis for Form-Based Codes**

Spikowski and Ohm (2006), in *Urban Lawyer*, have researched the legal basis for form-based codes. Many of our country's statutes are based on the 1926 Standard State Zoning Enabling Act (SSZEA) provisions. The "Grant of Power" provisions in the SSZEA include such elements as the "height, number of stories, and size; lot coverage; yards, courts, and other open spaces; density; and, location and use of structures and land." (Spikowski and Ohm, 2006) Thus, under the SSZEA, the Grant of Power provisions explicitly consider "form of development; i.e., coverage, setbacks, height, number of stories, density, and location of structures." The "Grant of Power" also authorizes regulation by use of structures and land, and describes use in the broadest terms of commercial, residential, and industrial. "The list does not preclude the consideration of form in local land regulation." The "Purposes in View" provisions of the SSZEA include the following purposes of zoning regulation: that it be "In Accordance with a Comprehensive Plan; lessen congestion in the streets; secure safety from fire, panic, and other dangers; promote health and general welfare; provide adequate light and air; prevent overcrowding of land; avoid undue concentration of population; and, facilitate adequate provision of public requirements." (Spikowski and Ohm, 2006)

As such, these purposes do not limit the regulation of the use of land. One additional item in the SSZEA’s list of purposes appears relevant to form-based codes. “Such regulations shall be made with reasonable consideration, among other things, to the character of the district and its peculiar suitability for particular uses, and with a view to conserving the value of buildings and encouraging the most appropriate use of land throughout the municipality. This provision appropriately balances the use of land and the form of development, i.e., character of the district. Contrary to conventional belief, then, the SSZEA is not exclusively use-based; indeed, it does not arguably show a preference for use over form.” (Spikowski and Ohm, 2006)

In addition, form-based codes, like all land use regulations, must conform to substantive due process. Because they are part of the local government’s police powers, form-based codes must advance “legitimate governmental interests” that protect the public health, safety, and welfare. Form-based codes must also satisfy procedural due process. The code must contain sufficiently detailed and meaningful standards to clearly inform developers to what is expected while allowing sufficient discretion for the County to determine the approval of an application. If not, the code would be considered vague. Most form-based codes that are prescriptive can certainly pass this test.

## **THE BASIC ELEMENTS OF THE FORM-BASED CODE**

**Regulating Plan.** A map of the regulated area designating the locations where different building form standards apply.

**Building Form Standards.** Regulations of configuration, features, functions of buildings that define/shape the public realm.

**Public Space/Street Standards.** Specifications for the elements within the public realm (e.g., sidewalks, travel lanes, street trees, street furniture, etc.).

**Architectural Standards.** Regulations controlling external architectural materials and quality.

**Administration.** A clearly defined application and project review process.

**Definitions.** Glossary to ensure the precise use of technical terms.

The Form-Based Code would be designed to foster a vibrant residential and commercial center through a lively mix of uses—with retail areas, sidewalk restaurants, offices and other commercial uses at street level, overlooked by canopy shade trees, and upper story residences. Redevelopment could be regulated by the Form-Based Code to achieve the vision in the Blueprint Cumberland II Master Plan.

The Form-Based Code would need to be a legal document that regulates development and redevelopment by promulgating reasonable controls on building form—while employing more flexible parameters relative to building use and density. By providing more emphasis on form than use, the result is a safer, attractive and enjoyable public place (good streets, neighborhoods and parks) together with a healthy mix of uses that fit within the CCID’s vision. Using form over use, the form-based code yields building uses that are more natural and comfortable. The Form-Based Code provides for graphic depictions and measures for height, siting, and building elements to formulate those essential qualities of character and unforgettable public spaces. Redevelopment and development projects within the Form-Based Code area must also follow all other Federal, State or local laws.

- **The Regulating Plan**

To simplify the form-based code and make it more visual and applicable to different situations, some of the rules should be placed on the map. A Form-Based Code should have its regulating plan contain the build-to lines, arcade lines, and other characteristics displayed on each parcel. The regulating plan establishes the vision for development. The plan shows where form-based codes apply and it guides developers and builders in how to comply with those codes by providing easy-to-follow illustrations of build-to lines, projected building footprints, and the building types allowed on each site.

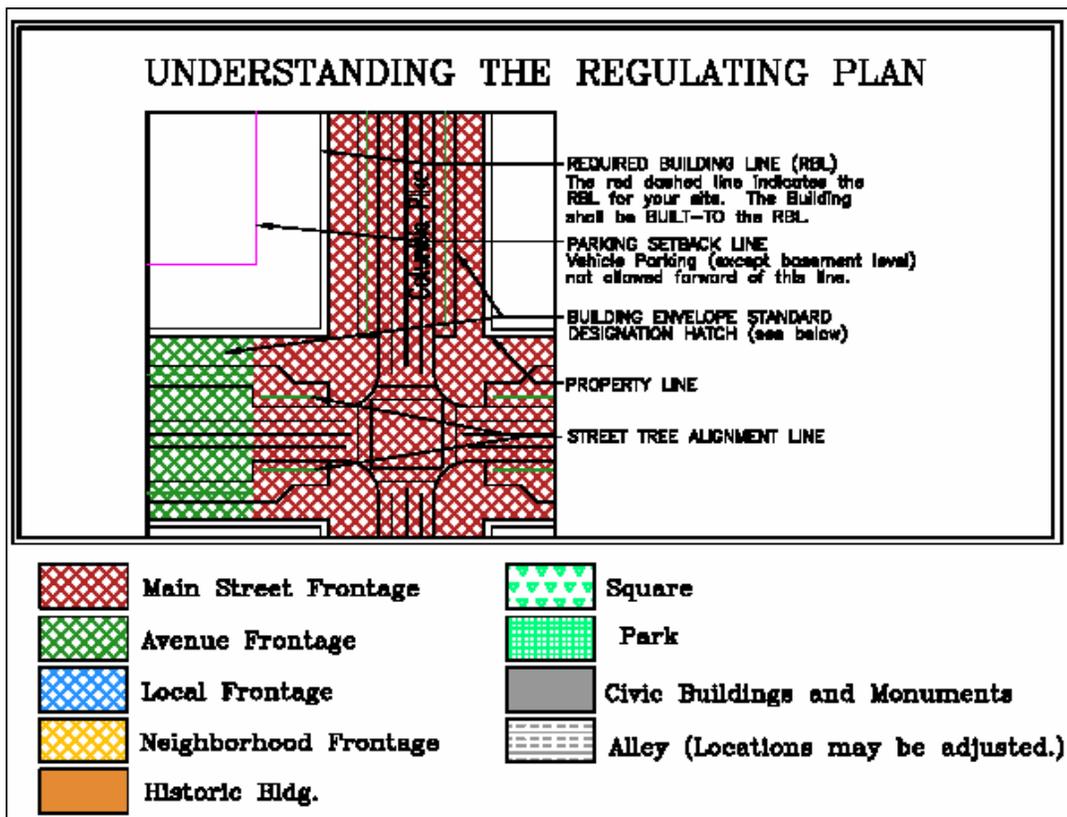
Form-based codes map the CCID into districts that differentiate the scale, form, and intensity of development they allow, rather than simply noting differences in allowable land use types. The regulating plan is the organizing principle for the zoning map in a form-based code rather than the use of residential, commercial, and industrial distinctions of traditional zoning.

Again, this Regulating Code is prescriptive – that is, it prescribes with detail, where a building must be placed on the lot, the general shape and size of the building, where parking areas and yards and courts must be located, and where various uses (a fairly wide range in the case of mixed-uses buildings) may be located within the building. To prepare such a Code, one must have a clear understanding of the sorts of buildings that the CCID would welcome.

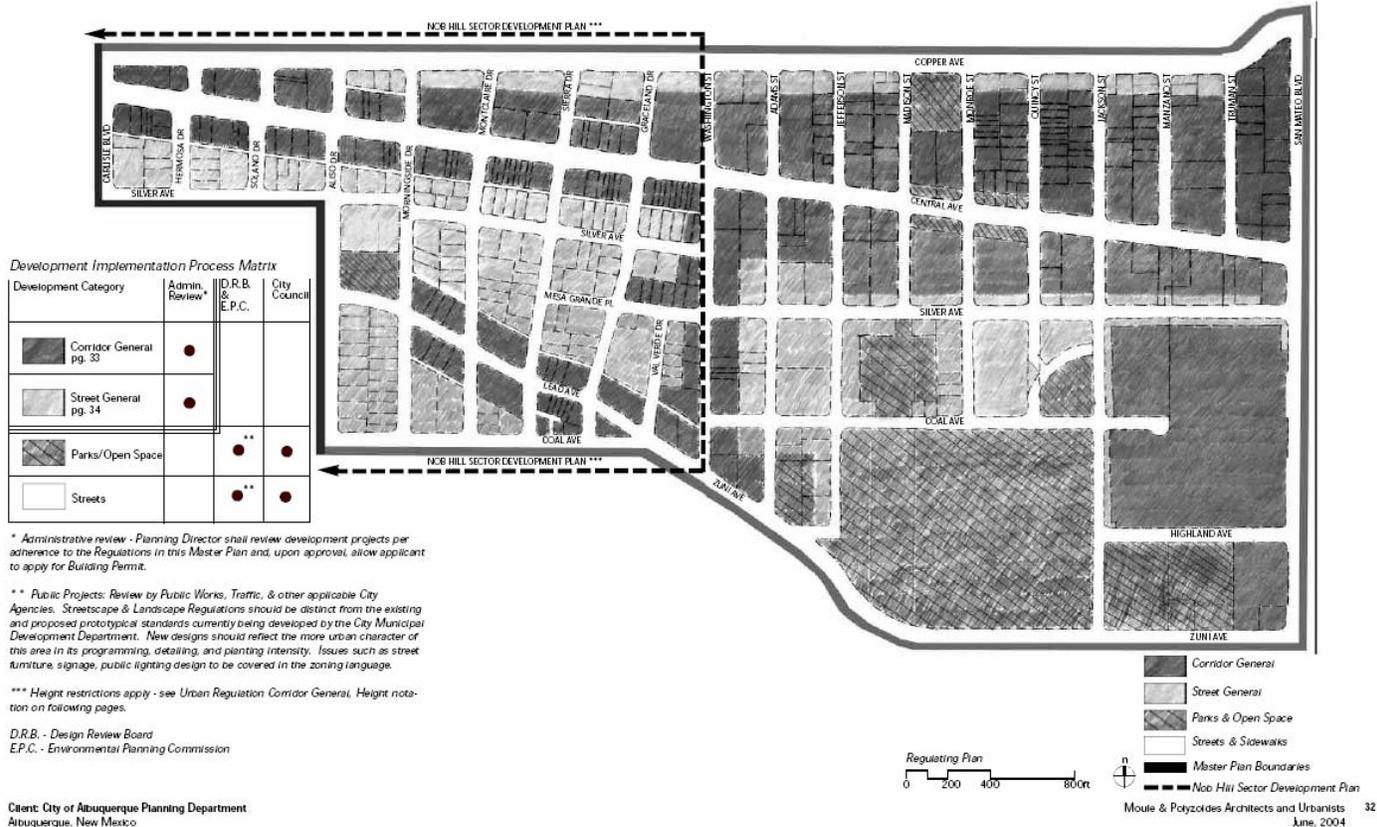
Each of the standards in a form-based code, regarding site layout and building design, would be formulated with consideration of how it will interact with other standards. This allows a determination how a building relates to other buildings on its sides and rear, and how a building relates to those across a fronting street to define the public realm of the street.

The standards are coordinated to collectively ensure that each increment of private development, and each feature and improvement within the public right-of-way all work together to produce a specific desired urban design outcome, or "vision" over time.

Coordination means, for example, that building height limits may be set based on the effect they will have on the pedestrian's experience of walking along the street, based on the total width of the street right-of-way, the sidewalk width, whether street trees exist or will be provided, and whether buildings are to be placed at the back of the sidewalk or set back. In regulating the location of buildings on property, form-based codes sometimes replace front and street side setback requirements with build-to lines.



**Sample Regulating Plan**



## Sample Regulating Plan



Corridor General Includes those areas that are appropriate for new development along commercial & retail-oriented streets. The setback, heights, massing, encroachments and parking arrangements would be appropriate to new, vibrant mixed-use buildings.

**1. MATERIALS & MISC.:**

**A. WALLS**

1. Brick- regional material, color, coursing, & detail
2. Stone - regional material, color, & coursing
3. Stucco - smooth or sand finish, warm colors, tone of 30% min., on grayscale.
4. Panels - 15% max. of main facade, of terra cotta, wood [paint/stain], enameled steel or steel.

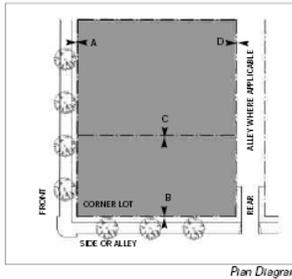
**B. OPENINGS**

1. Proportions - vertical
2. Horizontal openings may occur if vertical mullions provided at exterior side of door or window.
3. Glazing shall be clear, non-tinted, non-reflective. Public spaces @ grade shall have single pane glass at eye level. Ground floor building frontage shall be designed with 40%-50% of the building frontage length glazed, with the window sill no higher than 30" [inches].
4. Placement - openings shall occur along public street facades @ 30' [feet] o.c. min., openings to be 3' [feet] wide min..
5. Entrance- each ground floor use shall have 1 entrance min. for each 30' [feet] or less of building frontage length.

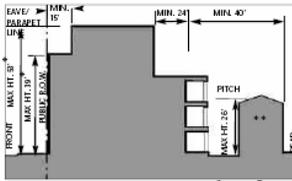
**C. MISC.**

1. Rooftop mechanical units shall be set back from roof edges. Units visible from public streets and/or homes shall be shielded from view with metal screens or parapet walls of wall materials listed above @ WALLS.
2. Articulation - building facade at front and side street shall change each 50' min. in height, or setback, or material.
3. Property walls & fences - allowed at fronts, sides & rears of bldgs. per current City regulations.
4. Drive-thru type buildings allowed only on rears of properties or on sides of properties [when adjacent to a side street].
5. Signage - per existing, applicable zoning category

**A. BUILDING PLACEMENT AND HEIGHT**



Plan Diagram



Section Diagram

**1. SETBACKS**

Buildings shall be placed within the shaded area as shown in the above diagram.

- A: Front Setback: 0'
- B: Side Street Setback: 0'
- C: Side Yard Setback: 0'
- D: Rear Setback: 0'
- \*\* 4th Story Front setback 15', Rear setback: 24'

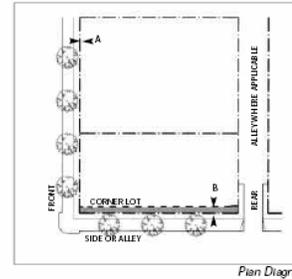
**2. HEIGHT**

Building height shall be measured in feet from average finish grade [on site] to top of parapet or midpoint of pitch. Maximum: 53'-0" Ht.

\*West of Carlisle Blvd., heights to be amended in the Nob Hill Sector Development Plan Area.

\*\* Optional: accessory building may occur this area

**B. BUILDING FRONTAGE AND PROFILE**



Plan Diagram



Section Diagram

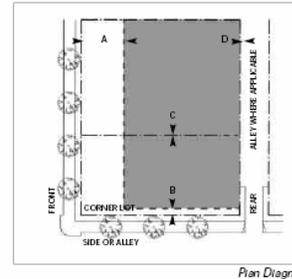
**1. ENCROACHMENTS ALLOWED:**

Arcades may encroach over the public R.O.W. at side streets. Encroachments into Public Right of Way [ROW] shall follow existing City regulations.

**2. FRONTAGE TYPES ALLOWED:**

Arcade, Shop front, Stoop, Forecourt - see Frontage types page 35.

**C. PARKING PLACEMENT**



Plan Diagram

**1. PARKING REQUIREMENTS**

On-site parking is allowed only in the shaded area as shown.

- A: Front setback: 30% of lot depth min.
- B: Side street setback: 10' min.
- C: Side setback: 0' min.
- D: Rear setback: 5' min.

Parking requirements below are subject to reductions due to mixed-uses and location on transit corridor:

- 1.5 parking spaces: residential or live/work [1 bathroom]
- 2 parking spaces: residential or live/work [2-3 bathrooms]
- 2.5 parking spaces: commercial or retail per 1000 s.f.
- 1 parking space: restaurants, per each 4 seats
- 1 parking space: per hotel room

Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings or solid 3'-0" min. high walls [all levels] or solid landscape at side streets and rear property lines.

**2. LANDSCAPE REQUIREMENTS:**

Landscape regulations should be distinct from the existing and proposed topographical standards currently being developed by the City Municipal Development Department. The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. Issues such as street furniture & public lighting design to be covered in the zoning language.

**3. PUBLIC RIGHT OF WAY IMPROVEMENTS:**

Sidewalks, trees, and other items related to the public ROW affected by private development to be reviewed by Design Review Board [DRB] during project review.

**Sample Regulating Plan Matrix and Standards**



The category Street General allows for new development of an intensity that compatible with both businesses and residences adjacent to it.

**1. MATERIALS & MISC.:**

**A. WALLS**

1. Brick - regional material, color, coursing, & detail
2. Stone - regional material, color, & coursing
3. Stucco - smooth or sand finish, warm colors, tone of 30% min. on grayscale.
4. Panels - 15% max. of main facade, or terra cotta, wood [paint/stain], enameled steel or flat steel [painted].

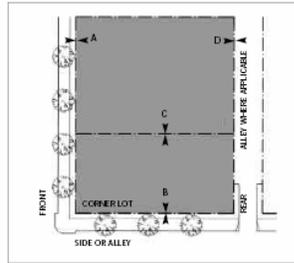
**B. OPENINGS**

1. Proportions - vertical
2. Horizontal openings may occur if vertical mullions provided at exterior side of door or window.
3. Ground floor building frontage shall be designed with 40%-80% of the building frontage length glazed, with the window sill no higher than 36" [inches].
4. Placement - openings shall occur along public street facades @ 30' [feet] o.c. min., openings to be 3' [feet] wide min.
5. Entrances - each ground floor use shall have 1 entrance min. for each 30' [feet] or less of building frontage length.

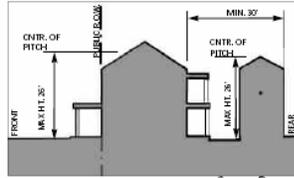
**C. MISC.**

1. Rooftop mechanical units shall be set back from roof edges. Units visible from public streets and/or homes shall be shielded from view with metal screens or parapet walls of wall materials listed above @ WALLS.
2. Articulation - building facade at front and side street shall change each 30' min. in height, or setback, or material.
3. Property walls & fences - allowed at fronts, sides & rears of bldgs. per current City regulations.

**A. BUILDING PLACEMENT AND HEIGHT**



Plan Diagram



Section Diagram

**1. SETBACKS**

Buildings shall be placed within the shaded area as shown in the above diagram.

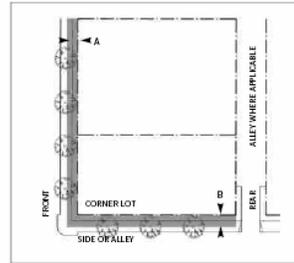
- A: Front Setback: 0', or matchsetback if adjacent lot is single family house or duplex.
- B: Side Street Setback: 0'
- C: Side yard Setback: 0'
- D: Rear Setback: 0'

**2. HEIGHT**

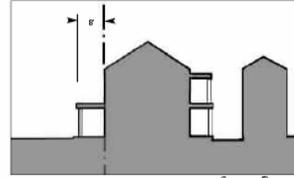
Building height shall be measured in feet from average finish grade (on site) to top of parapet or midpoint of pitch. Maximum: 26' Ht. max.

\*Optional: accessory building may occur this area.

**B. BUILDING FRONTAGE AND PROFILE**



Plan Diagram



Section Diagram

**1. ENCROACHMENTS ALLOWED:**

Frontage types may encroach into the setback as shown in the shaded area. Encroachments into Public Right of Way [ROW] shall follow existing City regulations.

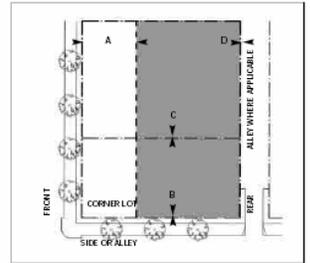
- A: Front encroachment: 8' max
- B: Side street encroachment: 8' max

Maximum encroachment height is 1 story.

**2. FRONTAGE TYPES ALLOWED:**

- Stoops [balconies, bay windows, & open porches].
- Forecourts, & front yards - see Frontage types page 35.

**C. PARKING PLACEMENT**



Plan Diagram

**1. PARKING REQUIREMENTS**

On-site parking is allowed only in the shaded area as shown.

- A: Front setback: 70% lot depth
- B: Side street setback: 10' min.
- C: Side setback: 5' min.
- D: Rear setback: 5' min.

Parking requirements below are subject to reductions due to mixed-uses and location on transit corridor:

- 1.5 parking space: residential or live/work [1 bathroom]
- 2 parking spaces: residential or live/work [2-3 bathrooms]
- 2.5 parking spaces: commercial or retail per 1000 s.f.
- 1 parking space: restaurants, per each 4 seats
- 1 parking space: per hotel room

Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings or solid 3'-0" min. high walls [all levels] or solid landscape at side streets and rear property lines.

**2. LANDSCAPE REQUIREMENTS:**

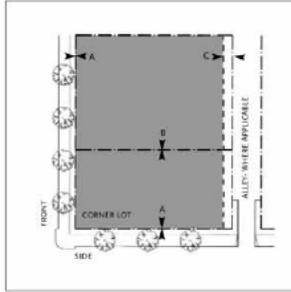
Landscape regulations should be distinct from the existing and proposed prototypical standards currently being developed by the City Municipal Development Department. The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. Issues such as street furniture & public lighting design to be covered in the zoning language.

**3. PUBLIC RIGHT OF WAY IMPROVEMENTS:**

Sidewalks, drives, and other items related to the public ROW affected by private development to be reviewed by Design Review Board [DRB] during project review.

**Sample Regulating Plan Matrix and Standards**

**A: Building Placement**



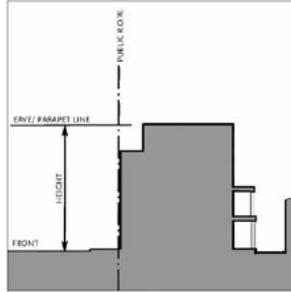
Plan Diagram

**1. SETBACKS**

Buildings shall be placed within the shaded area as shown in the above diagram.

- A: Street Setback: 0'
- B: Sideyard Setback: 0' min
- C: Rear Setback: 5' min

**B: Building Profile and Frontage**



Section Diagram

**1. HEIGHT**

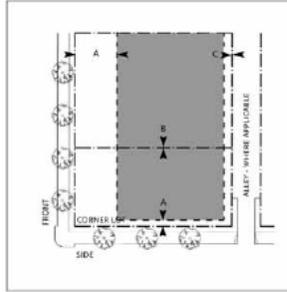
Maximum: 3.5 stories

**2. FRONTAGE TYPES ALLOWED:**

- Stoop
- Lightwell
- Forecourt
- Storefront
- Gallery
- Arcade

See Frontage Type definitions for descriptions, standards and design review considerations.

**C: Parking Placement**



Plan Diagram

**1. PARKING PLACEMENT**

On-site surface parking is allowed only in the shaded area as shown.

- A: Street setback;
- B: Side setback: 5'
- C: Rear setback: 5'

**2. PARKING REQUIREMENTS**

- Residential:
- 1 cars / unit
- Non-Residential:
- ParkOnce

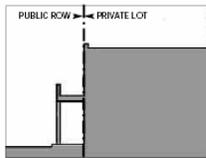
**D: Building Types**

	Lot Width					
	50'	75'	100'	125'	150'	200' +
Single Family						
Duplex						
Quadplex						
Eightplex						
Lane						
Mansion						
Bungalow Court						
Townhouse	Y	Y	Y			
Sideyard		Y	Y			
Live/Work	Y	Y	Y			
Courtyard (2 stories)						
Courtyard (2.5 stories)						
Courtyard (3.5 stories)				Y	Y	
Stacked Flats			Y	Y	Y	BL
Commercial Block			Y	Y	Y	
Blended						

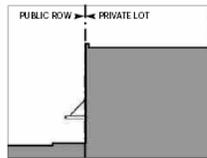
See Building Type definitions for descriptions, standards and design review considerations.

## Sample Regulating Plan Matrix and Standards

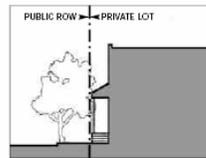
Frontage Types. Frontage Types are applied to each zone (Corridor General, Street General). These represent a range of additions to the basic facade of the building. In the following illustrations, "ROW" means the public street right-of-way.



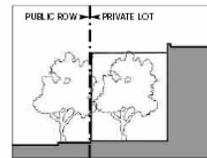
**A. Arcade:** the facade of a building with an attached colonnade. Balconies may overlap the sidewalk, while the ground floor remains set at the lot line. This type is ideal for retail use, but only when the sidewalk is fully absorbed within the arcade so that a pedestrian cannot bypass it. An easement for private use of the right-of-way is usually required. To be useful, the arcade should be no less than 8 feet wide clear in all directions.



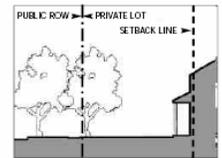
**B. Shop front:** the facade is placed at or close to the right-of-way line, with the entrance at sidewalk grade. This type is conventional for retail frontage. It is commonly equipped with cantilevered shed roof or awning. The absence of a raised ground floor story precludes residential use on the ground floor facing the street, although this use is appropriate behind and above.



**C. Stoop:** the facade is placed close to the frontage line with the ground story elevated from the sidewalk, securing privacy for the windows. This type is suitable for ground-floor residential uses at short setbacks. This type may be interspersed with the shop front. A porch may also cover the stoop.



**D. Forecourt:** the facade is aligned close to the frontage line with a portion of it set back. The resulting forecourt is suitable for gardens, vehicular drop offs, and utility off loading. This type should be used sparingly and in conjunction with the stoops and shop fronts. A fence or wall at the property line may be used to define the private space of the yard. The court may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the court.



**E. Front yard:** the facade is setback from the frontage line with a front yard. An encroaching porch may also be appended to the facade. A great variety of porch designs are possible, but to be useful, none should be less than 8 feet deep and 12 feet wide. A fence or wall at the property line may be used to define the private space of the yard. The front yard may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the yard.



### Example of Frontage Standards within Form-Based Code



### Example of Frontage Standards within Form-Based Code

The building envelope standards ensure that new development is consistent with the character of the zone by prescribing building heights, placement, and orientation. The location of a building is regulated by the build-to-line. A Required Build-to-Line (RBL) is the location on each parcel near the street right-of-way where building fronts are placed. It is used to develop a traditional main street or urban neighborhood with buildings that front sidewalks and help to create a pedestrian environment.

The Building Envelope Standards, in the Form-Based Code, establish basic parameters governing building form, including the envelope for building placement in three dimensions and certain permitted or required building elements, such as storefronts, balconies, and street walls. These standards also provide for both the boundaries within which development may take place and those requirements that apply. Building standards are determined by its street frontage contained in the Code's Regulating Plan. This produces a coherent street space and allows the building greater latitude behind its street facade. Building Envelope Standards shape vital public space throughout the CCID using placement and envelope controls on buildings.

The Regulating Plan would define and identify perhaps districts of the form-based code within the CCID. Standards regulate the features of buildings and streets that affect the public realm. These standards include building placement, height, and block creation, and vary according to the zone applied to parcels through the Regulating Plan. Proposed development and new land uses shall comply with the applicable standards contained in this Form-Based Code.

The matrix will identify the land use types allowed in each of the districts established by the Regulating Plan. Each parcel shall be occupied only by uses allowed within the applicable district subject to the type of approval required by the matrix and shall be compatible with existing uses.

Land uses that were lawfully established and existed within the Form-Based Code area as of the effective date of the Form-Based Code are allowed to continue to operate pursuant to the applicable provisions of the zoning code. All new development and redevelopment would be governed by a Form-Based Code, once adopted.

For example, a Retail/Town Center (RTC) District might incorporate areas within the CCID appropriate for use as a metropolitan level mixed-use center. A wide variety of uses could be allowed, including residential towers. Each of the RTC uses promote a central “main street” with active ground floor uses including retail, offices, and restaurants and with upper floors accommodating offices or residential. Lodging, entertainment, and civic uses are also encouraged. The RTC would include maximum building height standards.

Another district could contain housing units and would be an Urban Neighborhood (UN) District. This District would incorporate portions of the CCID appropriate for sustainable residential neighborhoods, employment, and mixed-use blocks in multi-story buildings. A UN District would be used for residential projects to cluster in nodes around local services. Small scattered residential projects would not be a part of the UN District. Mixed-use would be appropriate with ground floor uses including residential, retail, offices, and restaurants, and upper floors accommodating offices or residential.

The Form-Based Code might also incorporate a Multiple Use (MU) District for more contemporary types of development. The MU District is characterized by a horizontal or vertical mix of uses within a campus of multiple buildings. Opportunities for future intensification include freestanding residential or ground floor retail, offices, and restaurants, with upper floors accommodating offices or residential. Lodging, entertainment, and civic uses would also be a part of the MU District

It is necessary to include an Office Complex District. The intent of the Office Complex (OC) District would be to maintain the existing commercial character of the CCID, consistent with the Blueprint Cumberland II goal of protecting existing businesses.

One or more land uses identified as allowed may be established on any parcel within a District subject to the requirements listed in the matrix and in compliance with all applicable requirements in this District. Ground floor commercial uses may include the non-residential portion of live/work units, or may be designed so that the space may be used for either residential or non-residential uses where the residential portion does not face the designated street. Ground floor commercial uses shall have a minimum depth of interior floor area. A commercial land use may be combined with a residential land use within a dwelling unit to create a space that contains both a residence and commercial use area.

- **Public Space/Street Standards**

Urban design promotes streets as unifying public spaces so that compatible developments will face one another across those streets. Coding by street type is well-suited for urban redevelopment areas, since the width of parcels can vary. This is a very important point, for the regulations control the relationship of the street to the building. However, the provisions must also provide for the locations for parking. A hierarchy of street types would be used to organize the form-based code. Coding by street types is quite appropriate for the Cumberland CID area.

The street standards not only define public spaces, they also prescribe street networks and block patterns and hardscape and landscape materials such as tree species and pavers, the types of lighting, and pavement. The purpose of the Streetscape Standards, within the Form-Based Code, is to assist builders and developers with framing the relationship between the public spaces and their buildings. These street standards depict and explain the placement of the streetscape elements on or near each parcel being developed.

The Street-Type Specifications in the Form-Based Code indicate typical configurations for streets. These specifications address vehicular traffic lane widths, curb radii, sidewalk and tree planting area dimensions, and on-street parking typical sections.

There will be standards for residential and mixed-use block size. Maximum block size criteria would be established to create and maintain a connected network of streets that improves connectivity, walkability, emergency access, and a variety of building types. Blocks would have a long dimension and a short dimension. If either dimension is exceeded, a new street should be added so that its block size is not greater than those dimensions. Street types that define and create the block sizes include existing public arterial streets, existing public collector streets, existing public local streets, and new private streets used for service and emergency access.

Streets should be aligned and located to produce a connected street pattern. New streets link to other local, collector, and/or arterial streets. When new streets cannot connect to arterials due to intersection spacing constraints, the street could end before the arterial with adequate vehicular turnaround area, and continue as a pedestrian walkway connecting to the arterial adjacent sidewalk. The code could also provide for a “walking street” in such cases.

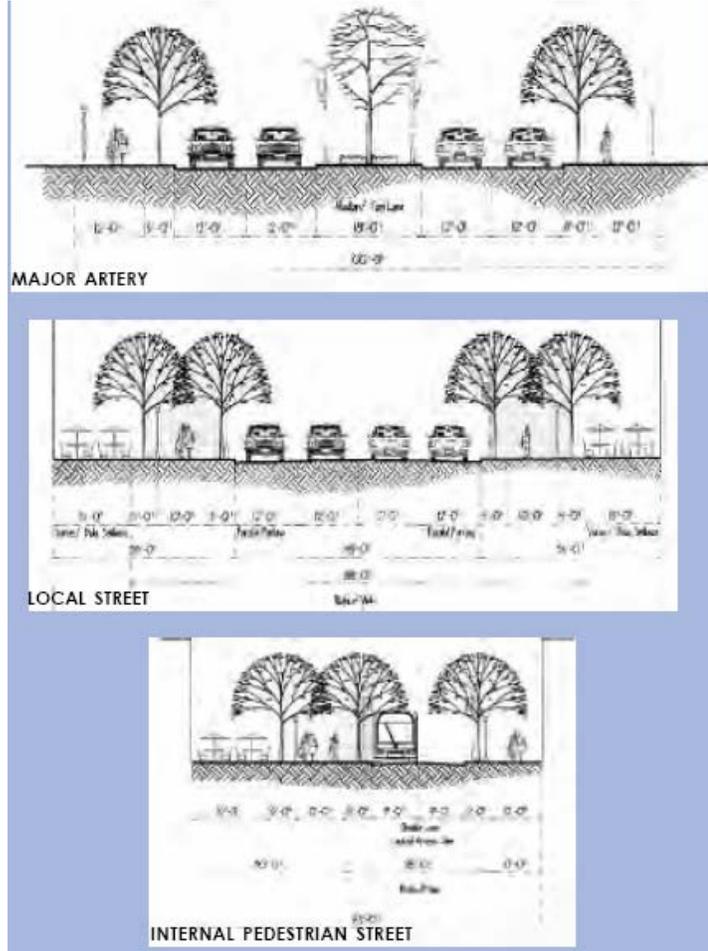
To provide adequate emergency and public access, development sites have minimum frontages along public or private streets. However, a private service street or walking street is not a frontage street.

Street standards apply to all Districts in a Form-Based Code. For instance, an RTC District would be based on access to the arterial transportation network and nearness to transit services. Within the RTC, some parts of roadways are designated Retail-Town Center “streets” and promote uses into that portion of adjacent buildings that are oriented to the street uses that qualify in the matrix as ground floor commercial. Street frontages throughout the RTC district need to be pedestrian-oriented on smaller blocks. Urban streetscapes on these roadways should be more pedestrian-friendly as they go through or if they are adjacent to the RTC district.

New, smaller, non-arterial streets, as part of a grid network, are used within the Urban Neighborhood (UN) district. These streets would be pedestrian-oriented with highly articulated residential frontages. The roads include residential and non-residential building facades and have lush, dominant landscape.

Street frontages throughout the Multiple Use (MU) district are also pedestrian-oriented. Streetscapes provide continuity and connectivity throughout the MU areas. New streets have smaller blocks.

To promote an active street life and provide essential local services, ground floor commercial uses would face those streets identified as a Retail-Town Center “street” on the Regulating Plan. Ground floor commercial uses would also be used along all other streets within the Form-Based Code districts.



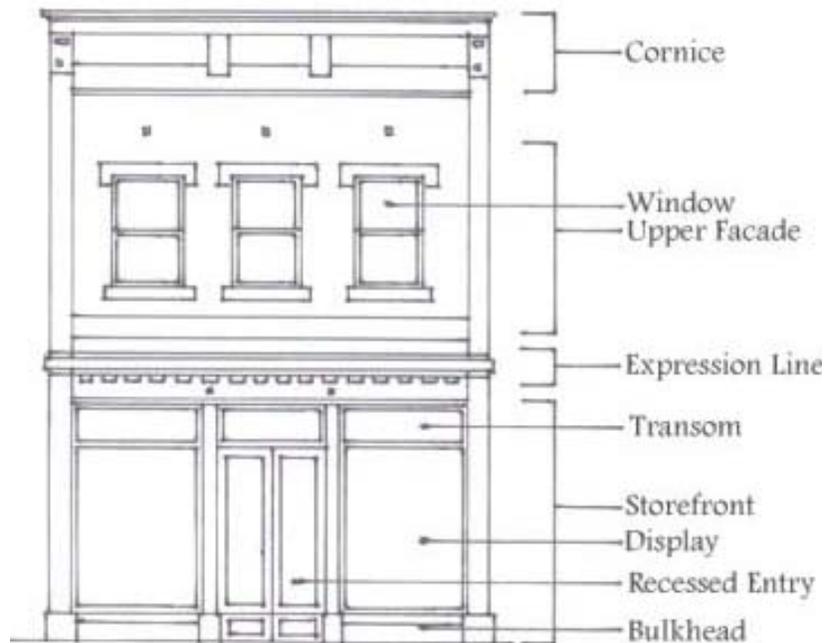
**Example of Typical Sections for Street Network**

- Buildings are aligned and close to the street and form the space of the street.
- The street has consistent building forms on both sides and contributes to a clear public space and street space identity.
- Buildings oversee the street space with active fronts providing a vital and safe public space.
- Public spaces are physically defined by buildings, walls, or fences with land in public view and under surveillance or private and protected.

- Buildings are designed for views directed to the street space and interior gardens/court-yards, not into neighboring parcels.
- Vehicle storage/parking, (other than on-street parking), garbage and mechanical equipment are kept away from the street space.

- **The Architectural Standards**

The architectural standards provide detail on walls, fences, arches, columns, roofs, windows, and for building materials and colors. Diagrams pertaining to each of these architectural details are included to give developers and builders an idea of what should be built. The Form-Based Code's architectural standards promote a harmony of design without requiring approval of every design detail.





**Stakeholders' Preferred Architecture Forms, Blueprint Cumberland I Report**



**Stakeholders' Preferred Architecture Forms, Blueprint Cumberland I Report**

- **Nonconforming Uses and Structures**

Also, the Form-Based Code would regulate those nonconforming uses and structures that are determined to be incompatible with the uses and form dimensions as specified in the Form-Based Code.

- **Development Review Committee**

The Form-Based Code includes a Development Review Committee (DRC) to coordinate and centralize the technical review of all plans to ensure compliance with the provisions of the Code. The Development Review Committee includes representatives from the departments that have an interest in the site plan and development review and approval process. The Committee is responsible for reviewing site plans, as well as concepts, and any other development-related applications that may be required under the Form-Based Code. It reviews applications and interprets the standards and requirements outlined in the Code.

## **FORM-BASED CODES GLOSSARY OF TERMS**

### **Alley:**

The vehicle passage-way within the block that provides access to the rear of buildings, vehicle parking (e.g., garages), utility meters, recycling and garbage bins.

### **Arcade:**

A roofed or built structure, extending over the sidewalk or square, open to the street except for supporting columns, piers, or arches. Residential or office units may occupy the space over the arcade. Arcades shall have, at the sidewalk (street), a minimum clear height (signage or lighting may encroach) and a minimum clear width (from frontage or Required Building Line (RBL) to inside column face). The area within an arcade shall be open to all public access. A supporting column/pier shall be located a fixed distance from the back of the curb. Where an arcade is built, the requirement for street trees is waived for that street frontage.

### **Architectural (Building) Type:**

A structure defined by the combination of configuration, placement and function.

### **Balcony:**

The exterior platform attached to the front of the main building (the Required Building Line (RBL) or street side). Required balconies, as defined in the Building Envelope Standards, must be roofed and enclosed by balustrades (railings) and posts that extend up to the roof and shall not be otherwise enclosed above a fixed height except with insect screening. Balconies aligned vertically on adjacent floors may post up to one another and share a single roof element.

### **Block:**

The aggregate of private lots, passages, common drives and, lanes, circumscribed by thoroughfares.

### **Buffer:**

Physical separation between uses.

**Buildable Area:**

The area of the lot within which buildings will sit. The Buildable Area sets the limits of the building footprint now and in the future -- additions must be within the designated area. This provides for construction in a manner consistent with the Cumberland CID urban design standards.

**Building Corner:**

This refers to the outside corner of a building (where the building mass is within an angle less than 180 degrees. Some of the proscriptions of the Building Envelope Standards are specific to building corners. Inside corners, where the exterior space is within an angle less than 180 degrees, are not considered building corners.

**Building Envelope Standards:**

The Building Envelope Standards establish the basic parameters governing building construction. This includes the envelope for building placement (in three dimensions) and certain required/permitted building elements, such as balconies and street walls.

**Civic Use:**

The term defining not-for-profit organizations dedicated to the arts, culture, education, government, transit, and municipal parking facilities. Civic uses are community uses open to the public including: meeting halls, libraries, schools, child care centers, police stations, fire stations, post offices (retail operations only, no primary distribution facilities), religious halls, museums, cultural societies, visual and performance arts, transit centers (including transit stations), and government functions, especially those involving the public. Civic use is an allowed use for any site.

**Civic Buildings and Monuments:**

Civic Buildings and Monuments are those structures located on the sites designated on the Regulating Plan and include any transit station structures. Other than location, they are not governed by this Code. The architecture of Civic Buildings and Monuments should reflect the citizens' highest aspirations for Cobb County.

**Civic Space:**

An open area dedicated for public use, typically for community gatherings. Civic Spaces are defined by the combination of certain physical constants defined by the relationship between their intended use, their size, their landscaping, and their enfronting buildings.

**Colonnade:**

A series of columns similar to an arcade, but spanned by straight lintels rather than arches, linked together, usually as an element of a building.

**Common Lot Lines:**

Lot lines shared by private lots, generally side lot lines.

**Curb:**

The edge of the vehicular pavement detailed as a raised curb or a swale. The curb usually incorporates the drainage system.

**Design Speed:**

The velocity at which a thoroughfare can be comfortably driven without the constraints of signage or enforcement. There are four ranges of speed: Very Low: below 25 mph, Low: 25-35 mph, Moderate: 35-40 mph and High: 45 mph and above. This factor determines the character and context for a particular segment of the thoroughfare system.

**Dormers:**

Dormers are permitted and do not count against the building story-height restrictions, so long as they do not break the main eaves line, and are individually less than 15 feet wide and collectively less than 30% of the unit's Required Build-to-Line facade.

**Elevation (Building):**

The exterior walls of a building not along a frontage. Also, it is referred to as 'Facade' when the elevation is along a frontage line.

**Entrance (Principal):**

The principal point of access for pedestrians to a building. In the support of pedestrian activity, the Principal Entrance should address the frontage rather than to the parking.

**Façade:**

The exterior wall of a building that is set along a frontage line. Facades support the public realm and are subject to frontage requirements additional to those required of elevations.

**Fenestration:**

An opening in the building wall allowing light and views between interior and exterior. Fenestration is measured as glass area (including muntins; excluding mullions) for occupied buildings and as open area for parking structures.

**Flex space:**

An integrated residence and working space.

**Forecourt:**

A semi-public exterior space partially surrounded by a building and also opening to a thoroughfare. These spaces usually lead to a court, which is a private exterior space. It is often used as a vehicular entrance or drop-off, and its landscape may be improved with paving.

**Frontage Type:**

The architectural element of a building between the public right-of-way and the private property associated with the building. Frontage Types combined with the public realm create the perceptible streetscape.

**Garage Entry:**

An opening (with curb cut) in the building façade and or street wall where vehicles may enter the block interior for general parking and business servicing. A garage entry shall have a maximum clear height and clear width (those existing prior and shall not be sited within an established minimum distance of another garage entry on the same block.

**Key Business:**

Businesses requiring a buffer from residential uses and identified on the Regulating Plan.

**Liner:**

A building that conceals a larger building, such as a public garage, that is designed for occupancy by retail, service, and/or office uses on the ground floor, with upper floors also configured for those uses or for residences.

**Live/Work:**

An integrated residence and working space, occupied and utilized by a single household in a structure, either single-family or multi-family, that has been designed or structurally modified to accommodate joint residential occupancy and work activity.

**Loft:**

A two-story volume residence with a mezzanine. Second stories of lofts on the top level of a building shall be counted as a separate story. Exceptions to this definition are permitted as allowed by the County's Building Code.

**Lot Width:**

The length of the Principal Frontage Line.

**Mixed-Use Main Street:**

A Mixed-use Main Street is a street characterized by continuous active ground floor retail storefronts and convenient parking to encourage pedestrian activity.

**Moving Plaza:**

A temporary loading area for deliveries to a residential complex.

**Mullions:**

A wood or metal part used to structurally join two window or door units.

**Muntin:**

Applies to any short or light bar, either vertical or horizontal, used to separate glass in a sash into multiple lights. Also, it is called a windowpane divider or a grille.

**Net Developable Area:**

It is the area defined by blocks, which is not to remain for public uses such as plazas, squares, streets or streetscapes.

**Pedestrian Pathway:**

Pedestrian Pathways are interconnecting paved walkways that provide pedestrian passage through blocks running from street to street or interior block parking area. The easement width for these pathways shall be set with a minimum and the paved walkway with a minimum, except where specifically noted on the Regulating Plan, and should provide an unobstructed view through their length.

**Planter:**

The layer of the streetscape which accommodates street trees. Planters may be continuous or individual according to the thoroughfare and location within the neighborhood.

**Podium:**

A building type with a partially or fully submerged parking garage that uses the deck of the garage as the base of the building.

**Porch:**

An open air room added to the mass of a building with floor and roof, with no walls on at least two sides. Different from a balcony or deck as surrounds main entry; provides transition from the public space of the street to the private space of the dwelling unit.

**Principal Frontage Line:**

Those lot lines that coincide with a public frontage line. One shall be designated as the **principal frontage line**. Facades along frontage lines define the public realm and are therefore more highly regulated than the elevations that coincide with other lot lines

**Private Court:**

A space used to provide service access to a site.

**Private Way:**

A Private Way is a new street created to reduce the block size within the CCID.

**Public Space:**

Property (streets, alleys, civic greens, squares and parks) within the public domain within which citizens may exercise their rights.

**Rear-Loaded:**

A Rear Loaded residential building is one that has vehicular access from the back of the building accessible through a service way.

**Regulating Plan:**

The Regulating Plan is the coding key for the Building Envelope Standards that provide specific information for the disposition of each building site. The Regulating Plan also shows how each site relates to adjacent public spaces, the overall CCID area and the surrounding environment. For parcels in special locations, there may be additional design guidelines identified in the Regulating Plan.

**Required Build-to-Line (RBL):**

The building must be built-to (coincident with) the Required Build-to-Line (RBL) The RBL is a requirement, not a permissive minimum as is a set-back. The RBL for each lot is shown on the Regulating Plan.

**Service Rooms:**

Residential rooms such as laundry rooms or closet, which do not serve as sleeping, dining, cooking or gathering rooms.

**Service Street:**

A Service Street is a street providing access to service and parking within a project, not a through street typically connected to the general street system.

**Shopfront Building:**

Shopfront Buildings are building types spatially defined in the Building Envelope Standard for shopfront building sites.

**Sidewalk:**

The paved layer of the public frontage dedicated exclusively to pedestrian activity.

**Square, Civic Green:**

Public Spaces located within the Cumberland CID. The Square is generally paved, appropriate to a more highly trafficked area. The Civic Green is a primarily unpaved, formally configured, small public lawn or park. Situated at prominent locations within the CCID, and often dedicated to important events or citizens, Civic Greens and Squares shall not include active recreation structures such as ball fields and courts.

**Street:**

Street includes all public space (streets, alleys, civic greens, squares and parks), including any transit station platforms, but not the transit line itself (or underneath), block interior (parking structure) driveways or alleys.

**Street Frontage:**

Street Frontage refers to the building line coincident with the Street Right-of-Way (ROW) or the RBL.

**Street Tree:**

A deciduous canopy shade tree as listed in the street tree list on the Regulating Plan. Street trees are of a proven hardy and drought tolerant species, large enough to form a canopy with sufficient clear trunk to allow traffic to pass under unimpeded and meet clear zone requirements when placed on state roads.

**Street Tree Alignment Line:**

A generally straight line that street trees are to be planted along. This alignment is parallel with the street or square and unless otherwise specified in the Regulating Plan is set a fixed distance from the back of the curb.

**Street Wall:**

A masonry wall built on the RBL or building line.

**Walking Street:**

A pedestrian connection used to create smaller blocks in the CCID.

## **STREETSCAPES MAINTENANCE PLAN DISCUSSION**

The Cumberland Community Improvement District has several considerations that would protect the investment of new streetscapes within the CCID boundaries. Because the Georgia Department of Transportation and the Federal Highway Administration require that the Cobb County Department of Transportation execute a maintenance agreement with the Georgia DOT for those streetscape projects, either the County, the CCID or a combination of both entities must be involved in maintaining those streetscapes.

As new developments/redevelopments occur within the CCID, those properties fronting on existing and/or planned streetscaped streets should be required to install streetscape improvements that meet the CCID urban design standards. These properties would become part of the maintenance plan.

Currently, property owners are required to maintain those existing streetscape improvements within the rights-of-way along their property.

As projects expand throughout the Cumberland Community Improvement District, alternate streetscape maintenance plans might need to be considered. Among these might be:

1. The Cobb County Department of Transportation maintain all streetscapes within the Cumberland CID, as part of the department's normal maintenance plan throughout Cobb County, and in accordance with the county's maintenance agreements with the Georgia DOT for those projects funded by the Georgia DOT.
2. The Cumberland CID could enter into an agreement with the Cobb DOT to maintain, from the CCID's budget, all streetscapes within its District boundaries and in accordance with the maintenance agreement between the Cobb DOT and the Georgia DOT.

3. The Cumberland CID could manage the maintenance of the streetscapes, but those owners adjacent to the streetscape provide that maintenance on a plan agreeable to the Cumberland CID Board. The CCID would maintain streetscapes not maintained by any property owners and the County, on the CCID's behalf, and ask those property owners to voluntarily reimburse the CCID for those maintenance costs incurred by it.
4. The Cobb County Commission, with the advice of the CCID, could form a Streetscape/Landscape Maintenance District, and assess each property owner within the Maintenance District boundary his or her fair share of the costs of maintaining the streetscapes according to a specified plan of maintenance.

The maintenance covered by the selected plan would include:

- For turf areas: mow, edge, aerate, fertilize, adjust/check/repair irrigation system, weed control, litter pickup, and clean hardscape areas.
- For groundcover areas: prune, edge, apply post/pre-emergents & plant growth regulators, fertilize, mulch, add soil amendments, litter pickup, adjust/check/repair irrigation system, weed control, and dead-heading (removal of dead blooms).
- For shrub & tree areas: structural pruning, pest/disease control, sight/sign clearing (removal of branches or trimming shrubs that might impair line of sight when driving or blocks signage), fertilize, mulch, add soil amendments, clean hardscape areas, adjust/check/repair irrigation system, apply post/pre-emergents, litter pickup, staking/bracing/removal, and weed control.
- Sidewalk sweeping and emptying of trash receptacles on a routine basis.
- Replace/repair permanent public information signs and gateway signs in the CCID.
- Repair/remove/replace of broken tree grates that pose a safety risk to pedestrians.
- Provide maintenance for sidewalk, public right-of-way hazards (repair or replace).
- Paint and/or repair streetscape benches and other street furniture.

- Providing working electrical outlets for special events.
- Conducting monthly inspections and checking for burnt bulbs and replacing burnt bulbs as necessary.
- Provide banners and flags for special events within the CCID.
- Prepare a monthly written report on maintenance needs and issues in the CCID.

## **Conclusion**

The Cumberland Community Improvement District has expended countless hours in the planning and decision-making process to ensure that investments of the stakeholders within the District continue to mature. In fact, the CCID has spent millions of dollars in planning and constructing streetscape improvements throughout the District to provide an interconnecting network of paths, trails and sidewalks to improve mobility and enhance the physical character and appearance of the District. The CCID wants those who work, live, and visit to have a positive memorable experience.

Creation of a long-term mechanism for continued maintenance and improvement of the physical character of the CCID is critical to the successful implementation of these goals. Without proper maintenance, the investments and resulting tangible and intangible benefits provided by the streetscape improvements will be lost over time. An effective maintenance program is essential to protecting the CCID's major investment in its streetscape program.

### FIVE (5) YEAR ACTION PLAN

Facility	Action Item	From/At	To	Project Type	Total Project Costs	Responsible Party	CID Cost (Local Match)	Funding Program
<b>US 41 (Cobb Parkway)</b>	Extend northbound left turn lane	Hargrove Road	N/A	Intersection Improvement	\$65,000	Cumberland CID	\$13,000	CID/CDOT
	Restripe the I-285 off ramp from 2 left turn lanes and 3 right turn lanes to 3 left turn lanes and 2 right turn lanes	I-285 eastbound off ramp	N/A	Intersection Improvement	\$5,000	Cumberland CID	\$1,000	CID/CDOT
	Add concrete barrier to separate left turn lanes and through lanes between intersections	I-285 westbound off ramp	Spring Road	Intersection Improvement	\$190,000	Cumberland CID	\$38,000	CID/CDOT
<b>Cumberland Boulevard</b>	Add an eastbound through lane, southbound right-turn lane, and restripe northbound approach to provide a separate left turn lane	Akers Mill Road/Stillhouse Road	N/A	Intersection Improvement	\$470,000	Cumberland CID	\$94,000	CID/CDOT
<b>Akers Mill Road</b>	Akers Mill Road West	Cumberland Boulevard	US 41 (Cobb Parkway)	Pedestrian Improvement	\$341,000	Cumberland CID	\$68,200	LCI/CID
	Akers Mill Road East	West of I-75	Cumberland Boulevard	Pedestrian Improvement	\$247,000	Cumberland CID	\$49,400	LCI/CID
	Akers Mill Road Pedestrian Bridge	Akers Mill Road/ Cumberland Boulevard	Walton River/ Potential Future Residential Redevelopment	Pedestrian Improvement	\$621,000	Cumberland CID	\$124,200	LCI/CID
<b>Akers Mill Road, Mill Green Parkway, Interstate North Circle/Parkway</b>	Akers Mill/ Interstate North	Akers Mill Road/ Cumberland Boulevard	Windy Ridge Parkway	Pedestrian Improvement	\$859,000	Cumberland CID	\$171,800	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard West	Spring Road	Akers Mill Road	Pedestrian Improvement	\$880,000	Cumberland CID	\$176,000	LCI/CID

<b>Cumberland Boulevard</b>	Cumberland Boulevard Southeast	Akers Mill Road	US 41 (Cobb Parkway)	Pedestrian Improvement	\$222,000	Cumberland CID	\$44,400	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard Northeast	Overton Park South	Akers Mill Road	Pedestrian Improvement	\$154,000	Cumberland CID	\$30,800	LCI/CID
<b>Cumberland Boulevard</b>	Cumberland Boulevard South	US 41 (Cobb Parkway)	Overton Park South	Pedestrian Improvement	\$489,000	Cumberland CID	\$97,800	LCI/CID
<b>Cumberland Boulevard and Windy Ridge Parkway</b>	Cumberland Boulevard/ Windy Ridge Parkway	Spring Road	Interstate North Circle	Pedestrian Improvement	\$1,805,000	Cumberland CID	\$361,000	LCI/CID
<b>Circle 75 Parkway</b>	Circle 75 Parkway	US 41 (Cobb Parkway)	Windy Ridge Parkway	Pedestrian Improvement	\$432,000	Cumberland CID	\$86,400	LCI/CID
<b>Spring Road</b>	Spring Road	Cumberland Boulevard	US 41 (Cobb Parkway)	Pedestrian Improvement	\$196,000	Cumberland CID	\$39,200	LCI/CID
<b>Interstate North Circle</b>	Interstate North Circle	Windy Ridge Parkway	Interstate North Parkway	Pedestrian Improvement	\$246,000	Cumberland CID	\$49,200	LCI/CID
<b>Interstate North Parkway</b>	Interstate North Parkway South	Windy Ridge Parkway	Interstate North Circle	Pedestrian Improvement	\$439,000	Cumberland CID	\$87,800	LCI/CID
<b>Interstate North Parkway/ Interstate North Parkway SE</b>	Interstate North Parkway North	Windy Ridge Parkway	Windy Hill Road	Pedestrian Improvement	\$446,000	Cumberland CID	\$89,200	LCI/CID
<b>Overton Park South, Overton Park North, Overton Park Drive</b>	Overton Park Area Pedestrian Circulation Improvements	Overton Park South	Akers Mill Road	Pedestrian Improvement	\$555,000	Cumberland CID	\$111,000	LCI/CID
<b>Riverwood Parkway and Cumberland Center IV/ Homewood Suites Driveway</b>	Riverwood Parkway	Cumberland Boulevard	Cobb Galleria Parkway	Pedestrian Improvement	\$600,000	Cumberland CID	\$120,000	LCI/CID
<b>US 41 (Cobb Parkway)</b>	US 41 (Cobb Parkway) North	Windy Ridge Parkway/ Cumberland Boulevard	Akers Mill Road	Pedestrian Improvement	Funded in TIP/RTP	Cumberland CID	Funded in TIP/RTP	STP Urban >200K
<b>US 41 (Cobb Parkway)</b>	US 41 (Cobb Parkway) South	Akers Mill Road	Cumberland Boulevard	Pedestrian Improvement	\$462,000	Cumberland CID	\$92,400	LCI/CID
<b>US 41 (Cobb Parkway) Pedestrian Bridge</b>	Pedestrian Bridge	Regent/Riverwood Mixed-use Development	Akers Mill Square	Pedestrian Improvement	\$181,000	Cumberland CID	\$36,200	LCI/CID
					<b>\$10,945,000</b>		<b>\$2,189,000</b>	

<b>PLANNING INITIATIVES</b>	<b>Description</b>	<b>Cost</b>	<b>Year</b>	<b>Responsible Party</b>	<b>Funding Source</b>
	Expand Blueprint Cumberland II master planning to include entire CCID area	\$200,000	2007	CCID	CCID
	Joint Corridor Master Plan with CCID, Fulton Perimeter CID and Sandy Springs	\$100,000	2007	CCID	CCID/FPCID
	I-75 Transit Plaza Feasibility and Concept Plan	\$100,000	2007	CCID	CCID
	Marketing Action Plan Implementation	TBD	2007	CCID	CCID
	Design and Implement gateways, signage and related improvements in CCID	TBD	2008	CCID	CCID
	Create a Pedestrian Mobility Action Plan--crosswalks, ped countdown signals, and plan to repair/replace/ and complete connectivity of sidewalks within CCID	\$50,000	2008	CCID	CCID
<b>OTHER INITIATIVES</b>					
	Engage Cobb County Housing Authority to assist in providing affordable and workforce housing	TBD	2007	CCID/Cobb Housing Authority(CHA)	CCID/Cobb Housing Authority
	Assure that the Cobb County Comprehensive Plan Update incorporates affordable and workforce housing elements	TBD	2007	CCID/Cobb County	CCID/Cobb County/CHA
	Engage developers and incentivize the provision of affordable and workforce housing as a part of their projects.	TBD	2007	CCID/CHA	CCID/CHA

## **STAKEHOLDERS GROUP & TECHNICAL GROUP**

The Stakeholders Group and Technical Team members have been meeting over the duration of the Blueprint Cumberland II project with consulting team members in an effort to combine opinions and knowledge to produce a consensus plan that is beneficial to all those involved. Numerous meetings throughout the project have been held and valuable information gained by the willingness of the Stakeholders Group and Technical Team members to share opinions and expertise. Stakeholders who regularly attended such meetings include:

<b><u>Name</u></b>	<b><u>Organization</u></b>
Jim Durrett	Livable Communities Coalition
David Knight	Walton Communities
Bob Ott	Cobb County Planning Commission
Michele Swann	Cobb-Marietta Coliseum and Exhibit Hall Authority/Cobb Energy Ctr.
Joe L. Thompson	Cobb Co Commission
Allen Jablonski	Terrell Mill Community
Connie Engel	Childress Klein Properties
Mason Zimmerman	Pope and Land
Amy Brey	Regent Partners, LLC
Jahnee Prince	Council for Quality Growth
Ron Sifen	Vinings Homeowners Association
Phillip Westbrook	Cobb Community Development
Trey Taylor	Congressman Tom Price's Office
Martha Adams	East Cobb Civic Association

Bill Cooper	Cobb Chamber of Commerce
Dan Pascale	General Growth Properties
Pat Hochstein	Worldspan
David Hong	East Cobb Civic Association
John Maximur	OLM, Inc.
Earl Shell	Hardin Construction
Allan J. Struletz	Mall Properties, Inc.
Barry Teague	Walton Communities

Representatives from the Technical Team were also present at these meetings and contributed to the project. The Technical Team consisted of representatives from the:

- Atlanta Regional Commission
- Smyrna Community Development
- Marietta Planning and Zoning
- Cobb County Community Development
- Cobb Department of Transportation
- Cobb County Economic Development
- Cumberland Community Improvement District

Without valuable input from these stakeholders and technical team members, the consulting team would not be able to create a comprehensive master plan. Their active involvement directly influenced the direction and shape of the Blueprint Cumberland II Master Plan. The combined effort of all of these assets has created the Blueprint Cumberland II Master Plan. This plan will take Cumberland/Galleria into a future of stable and prosperous urban growth.



**BLUEPRINT CUMBERLAND II**  
**TECHNICAL COMMITTEE**  
**KICKOFF MEETING AGENDA**  
Wednesday, July 19, 2006 @ 2 PM

1. WELCOME AND INTRODUCTIONS—TOM BOLAND
2. CONSULTANT TEAM OVERVIEW-JIM CROY, Croy Engineering
3. ISSUES FROM BLUEPRINT I—HARRY MCGINNIS, Croy Engineering
4. GOALS FOR BLUEPRINT II—BILL DE ST. AUBIN, SIZEMORE GROUP
5. LINES OF COMMUNICATION—JIM CROY
6. SCOPE AND SCHEDULE OF BLUEPRINT I—HARRY MCGINNIS
7. STUDY AREA—BILL DE ST. AUBIN
8. NEXT STEPS—TOM BOLAND

**Blueprint Cumberland II Stakeholders Meeting**  
**July 26, 2006; 10:00 AM – 12:00 PM**

The meeting was called to order by Malaika Rivers, Executive Director of the Cumberland Community Improvement District. All attendees identified themselves and the organization they represented. (A list of the attendees is included).

Jim Croy, President of Croy Engineering, gave an overview of the Blueprint Cumberland II project team consisting of Croy Engineering and the Sizemore Group. He then introduced team members present at the meeting. Ms. Rivers spoke briefly about the Blueprint Cumberland CID and its initiatives, provided data concerning its impact and put forth the question she wanted each of the participants to think about over the course of the meeting, “What do you see in the Cumberland/Galleria area right now and what would you like to see?”

Tom Boland, Planning Director of the Cumberland Community Improvement District gave an overview of the Blueprint Cumberland I. He reviewed the history of the project and the initial Blueprint Cumberland Strategic Plan. He also pointed out that each participant received a copy of the “Blueprint Cumberland Strategic Plan, Executive Summary”, “Blueprint Cumberland | 5 Year Action Plan Review | 2001-2006”, the Cumberland Community Improvement District Annual Report for 2005 and map of the area. Also each participant received a listing of the studies in the Cumberland CID Area and a timeline for Blueprint Cumberland II. The meeting was then turned over to Jim Croy.

Mr. Croy presented an overview of the Blueprint Cumberland II Study Process. The process will consist of the following elements:

- Initial Workshop – Vision and Goal Setting – Today’s meeting which included all the stakeholders, developers, property owners, CCID Board, County representatives and professional planners and architects.
- Preparation of Master Plans – Using the statements and goals from the workshop along with the issues and results of Blueprint Cumberland I and other studies, prepare a set of three master plans.
- Stakeholders Committee Plan Workshop – Present the three sets of plans to the Stakeholders Committee for review and comments.
- Preparation of Master Plan – Review the changes and comments from the Stakeholders Plan Workshop and prepare a single master plan.
- Technical Committee Plan Review – The Technical Committee will review the plan to ensure that it conforms to the CCID, County, and adjacent city plans, policies and goals.



- Cumberland CID Board Approval – The Consulting Team will present the selected master plan and final report to the Cumberland CID Board for its approval in late January, 2007.
- The CCID will present the results/recommendations to the Cobb County Commission for its approval.

After Mr. Croy's presentation, the Stakeholder Committee formed two groups to share thoughts and ideas on the current status of the Cumberland/Galleria area and where they want the area to be in the future. The groups followed a similar agenda where each group discussed the following four elements of the Cumberland CID area: strengths, weaknesses, opportunities and threats today and ideas for the future.

After the breakout sessions, the group came back to review the breakout session results. Some of the issues discussed were:

### **Cumberland/Galleria Today**

- Existing Strengths
  - Location
  - Access
  - Strong Leadership
  - Strong Infrastructure
  - Low Taxes
  - Redevelopment Plans already in place
  - Greenspace
  - Identity
- Existing Weaknesses
  - Aging rental properties
  - Lack of adequate neighborhood shopping opportunities
  - School system is inadequate
  - Employment/Residential ratio
  - Lack of transportation options
  - Congested interstate system
- Threats
  - Sandy Springs
  - Dobbins AFB
  - Perception of area
  - Image of Cumberland Mall
  - Highway expansion
  - Public safety

## **Cumberland/Galleria in 10 Years**

- Neighborhood churches, stores & schools
- Affordable housing
- Senior housing
- Mixed-use development
- Parks and Recreation, more greenspace
- Shuttle service within the area
- Strong marketing campaign
- Sense of community
- Town Center or focal point

After the break-out session results were presented, Jim Croy outlined the next steps in the procedure and mentioned that existing study results will be incorporated in the master plans presented, along with ideas and visions from the stakeholders present today. The meeting was adjourned at 12:00 PM. Meeting attendees were:

### **Stakeholder Committee Members**

Martha Adams, East Cobb Civic Association  
Amy Brey, Regent Partners, LLC  
Bill Cooper, Cobb Chamber of Commerce  
Dan Pascale, General Growth Properties  
Connie Engel, Childress Klein Properties  
Pat Hochstein, Worldspan  
David Hong, East Cobb Civic Association  
Allen Jablonski, Terrell Mill Community  
John Maximur, Livable Communities Coalition  
Tom Medlock, OLM, Inc  
Bob Ott, Cobb County Planning Commission, Board of Zoning Appeals  
Earl Shell, Hardin Construction  
Ron Sifen, Vinings Homeowners Association  
Allan J. Struletz, Mall Properties, Inc.  
Barry Teague, Walton Communities

### **CCID Representatives and Consulting Team**

Malaika Rivers, Executive Director Cumberland CID  
Tom Boland, Planning Director, Cumberland CID  
Jim Croy, Croy Engineering, LLC  
Harry McGinnis, Croy Engineering, LLC  
Mary Goodwin, Croy Engineering, LLC  
Bill de St. Aubin, Sizemore Group  
Venky Babu, Sizemore Group





## **BLUEPRINT CUMBERLAND II TECHNICAL ADVISORY COMMITTEE AGENDA**

**Wednesday, September 27, 2006 @ 10:00 AM**

- 1. Results of Meeting of July 19, 2006**
- 2. Status of Issues Analysis—Harry McGinnis/Venky Babu**
- 3. Case Studies—Bill De St. Aubin/Harry McGinnis**
  - **Pros and Cons**
  - **Planning Principles**
- 4. Form-Based Code Overlay District—Working Group Membership—Tom Boland/Harry McGinnis**
- 5. Next Steps—Tom Boland**



**Blueprint Cumberland II Stakeholders Meeting**  
**March 14<sup>th</sup> 2007, 10 AM – Noon; Cobb County Chamber of Commerce**

This meeting was called to order by Tom Boland, Project Manager of the Blueprint Cumberland II Project. Tom Boland, Director of Planning, Cumberland CID, gave a brief introduction and overview of the planning process which produced three separate Master Plans of the Cumberland/Galleria area. He then introduced Jim Croy, CEO of Croy Engineering. Mr. Jim Croy continued with an introduction of the consulting team members present at the meeting. In attendance were:

**Stakeholders/Technical Team Members**

Jim Durrett, Livable Communities Coalition  
David Knight, Walton Communities  
Bob Ott, Cobb County Planning Commission  
Michele Swann, Cobb-Marietta Coliseum & Exhibit Hall Authority/Cobb Energy Ctr.  
Joe L. Thompson, Cobb Co Commission  
Allen Jablonski, Terrell Mill Community  
Connie Engel, Childress Klein Properties  
Mason Zimmerman, Pope and Land  
Amy Brey, Regent Partners, LLC  
Jahnee Prince, Council for Quality Growth  
Ron Sifen, Vinings Homeowners Association  
David A. Jackson, Cobb DOT  
Phillip Westbrook, Cobb Community Development  
Trey Taylor, Congressman Tom Price's Office

**Consulting Team**

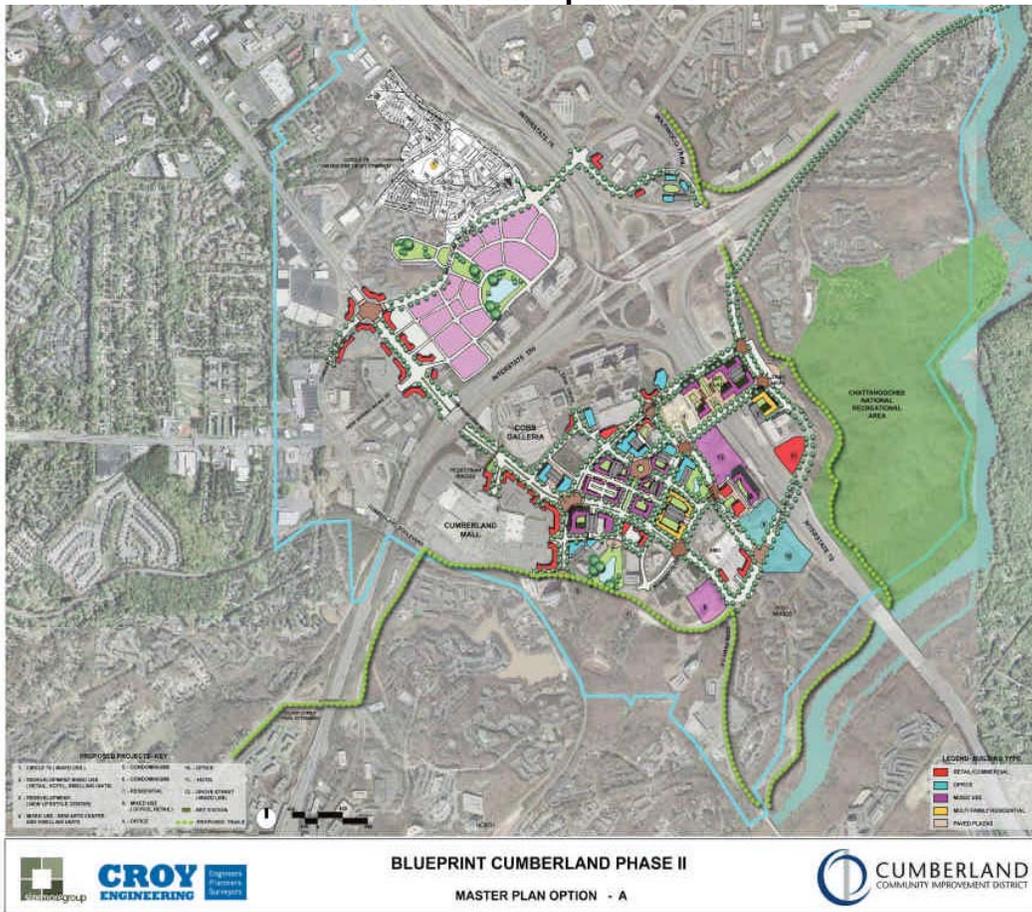
Tom Boland, Executive Director Cumberland CID  
Jim Croy, Croy Engineering, LLC  
Harry McGinnis, Croy Engineering, LLC  
Bill de St. Aubin, Sizemore Group

An overview of each of the three separate Master Plans was provided by Harry McGinnis of Croy Engineering and Bill de St. Aubin of the Sizemore Group. They stressed similarities and differences of the separate Master Plans to the stakeholders and technical team members. Following the overview, the stakeholders and technical team members were divided into three discussion groups and each group was assigned a different master plan. The groups were instructed to dissect their plan scenarios and list characteristics about that plan they would endorse and to identify concerns or problems. Each group presented its findings to the entire group of stakeholders.

The meeting was adjourned at 12:00 PM

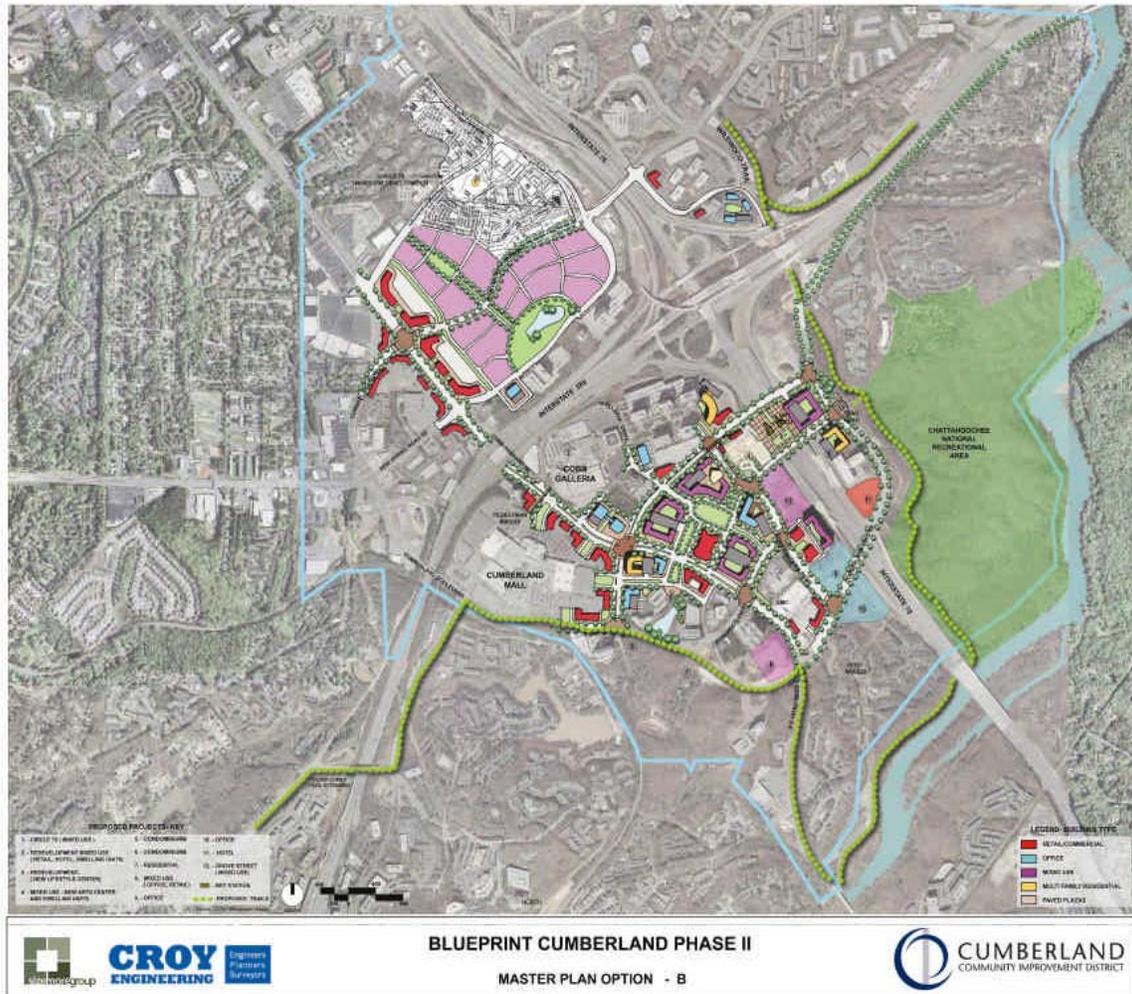


## Master Plan Option - A



- Preserve all streetscapes, new street connections and traffic calming measures.
- The study area has several assets/ anchors including the Cumberland Mall, Arts Center, Cobb Galleria, and the Chattahoochee National Recreation Area. The master plan should focus on circulation, connectivity and infrastructure improvement, rather than development of private parcels. The scheme is developed around Akers Mill Square as the central core, which is to be redeveloped in the near future.
- The detailed master plan should encompass detailed streetscaping and development along Cumberland Boulevard (the entire Loop road). Existing projects proposed along Cumberland Boulevard to the south can act as a catalyst for the development.
- Provide some recommendations for the larger impact area
- The connectivity to existing trail network is good
- Provide for signage/way finding – consistent design
- Provide for greenspace – passive recreational areas and pocket parks
- The TOD around transit station and other adjacent developments could be considered the “heart” of the study area, rather than concentrating on Akers Mill.
- Simplify intersections and provide for wider sidewalks
- Treat Cumberland Boulevard as a spine rather than an edge

## Master Plan Option - B



- Preserve the connection south of the transit station from Cumberland Boulevard to Cobb Parkway
- Preserve a central greenspace
- Preserve plans for transitional development south of Cumberland Boulevard
- The maximum densities should be concentrated around the transit station: High-rises @ transit station should transition down to mid-rises moving towards Cobb Parkway: Create mid-rise development on Akers Mill
- Retail on the corner of Cobb Parkway and Akers Mill
- Provide for Green Roofs
- Office development along Riverwood Parkway
- Mt. Wilkinson Parkway connection



**SUMMARY OF COMMENTS:  
POINTS OF CONSENSUS BETWEEN ALTERNATIVES – A, B & C TEAMS**

**Land Use:**

- Intense TOD with a public plaza around the transit station is an appropriate place for higher density uses and high-rise buildings
- Provide for housing diversity
- Interparcel connectivity
- Public parking decks

**Transportation:**

- A finer-grained street network (street grid) that connects the existing anchors/activity centers, provides multiple routes to a destination and provides the area with a more urban feel
- Preserve the connection south of the transit station from Cumberland Boulevard to Cobb Parkway
- Streetscapes and traffic calming measures on major thoroughfares
- Trail connectivity – link to existing trail network

**Greenspace:**

- Central usable greenspace/ town green: other passive recreational areas and pocket parks
- Trail connectivity to Chattahoochee National Recreation Area
- Preserve existing green areas, parks and streams
- Trees along the streets

## **SUMMARY OF COMMENTS: COMMENTS THAT CAME ONLY FROM ONE OF THE TEAMS**

### **Land Use:**

- The scheme is developed around Akers Mill Square as the central core, which is not up for redevelopment in the near future. Other areas like the parcels around transit and Arts Centre and areas along Cumberland Boulevard can instead be designed as catalysts for development
- Provide recommendations for larger impact area
- Office development along Riverwood Parkway (currently shown as mixed-use)
- Provide a quality grocery store

### **Transportation:**

- Develop a trolley system that connects all the different pods of development: It can be used by office crowds during daytime. During nights the trolley could carry residents to areas of activity (Arts Centre, restaurants etc.)
- Mt. Wilkinson Parkway connection
- Connect Columns Drive bike facilities to the Wildwood Trail and Perimeter CID area
- New proposed street south of TOD/Cobb Energy Performing Arts Centre that connects to the east side of I-75 (street over the I-75 freeway) to be only pedestrian connection not a vehicular street

### **Greenspace:**

- Provide for green roofs over parking decks

## **RECOMMENDED NEXT STEPS**

The following projects and initiatives are recommended for consideration by the Board of the Cumberland Community Improvement District. These recommendations are from the analyses conducted as a part of Blueprint Cumberland II, represent those issues that remain a continued concern to the CCID as a result of the initial Blueprint study, or are the direct result of those numerous other studies that have been conducted by local and state agencies that impact the CCID and the recommended master plan.

### **First Steps**

- 1. The Blueprint Cumberland II Planning Area, and the current master planning study, should be expanded to cover the area outside of the Core area and within the CCID boundaries.***
- 2. The growth taking place in both the Cumberland CID, the Fulton Perimeter CID, and the City of Sandy Springs necessitates closer cooperation in transportation planning. A joint transportation planning study should be conducted, as an expansion of Blueprint Cumberland II, to determine the long-term transportation needs of the affected area to properly plan for growth and seamless regional transportation networks.***
- 3. Using the results and recommendations of Blueprint Cumberland II, the Cumberland CID should establish an updated five (5) year list of priority projects.***
- 4. Blueprint Cumberland II recommends a public plaza integrated with the transit station that spans over the I-75 and connects the Core Area to the west with Cumberland Boulevard to the east. The area around the transit station is proposed to be developed into a Transit-Oriented Development (TOD) with higher density and diverse uses and activities. This includes a connection south of the Cobb Energy Performing Arts Centre in front of the transit plaza to link Cumberland Blvd to the west side of I-75.***

- 5. The CCID needs to conduct a study of recommendation #4 to determine the feasibility of locating the public plaza and street connection (parallel to Akers Mill Road) over I-75.**
- 6. To further the recommendations of the marketing study, the CCID should continue to aggressively pursue its action plan to promote the Cumberland CID area to the predetermined business and consumer targeted audiences.**
- 7. Establishing a physical identity for the Cumberland/Galleria community has been noted by the Cumberland CID as a priority concern to the CCID Board and its stakeholders. The Cumberland CID, in cooperation with the Cobb DOT, should identify funding and soon thereafter begin the implementation of a system of street signs, gateway signs and related gateway improvements, and wayfinding signs using the newly adopted Cumberland CID branding and logo.**
- 8. To facilitate greater mobility for pedestrians within the Cumberland CID, particularly the Core Area, the CCID should create a Pedestrian Mobility Action Plan that includes:**
  - the location of additional pedestrian bridge crossings;**
  - the location and design for pedestrian crosswalks at all intersections and pedestrian countdown signals at key intersections; and**
  - the location of sidewalks in need of repair, replacement or where connectivity is needed between land uses.****The Pedestrian Mobility Action Plan will include locations, conceptual designs, and implementation strategies (costs, funding options, responsible parties, etc.) for each element of the plan.**

## Second Steps

9. As large parcels are redeveloped, the Cumberland CID and Cobb DOT should promote the creation of street grids for improved mobility, pedestrian-oriented streetscapes and access management controls within the new development.
10. The Cumberland/Galleria Area would benefit from a master plan that promotes a distinctive architectural character and a vibrant discernable activity center - a *“focal point.”* Blueprint Cumberland II recommends a new focal point in the Cumberland/Galleria Core area with a linear town green/public plaza and space for public passive and active recreation with an array of mixed-uses; retail, office, entertainment, apartments, condos, townhomes surrounding it. This new activity center will create a sense of place and character for the community and will also connect the existing activity nodes and centers within the area; Cobb Energy Performing Arts Centre, Chattahoochee National Recreation Area, Cumberland Mall and the Cobb Galleria.
11. As identified in the review of planning tools used by peer communities and business districts throughout the U.S., the Cumberland CID should work closely with Cobb County Community Development to investigate the application of a form-based overlay district to govern land use and zoning within the CCID’s boundaries.
12. Mobility around the Cumberland CID area continues to be a concern to stakeholders. There continues to be a need for transit services within the Blueprint Cumberland II Core Area.
13. As the Cumberland CID continues to evolve, greater diversity in housing is essential to meet the demands of the marketplace, particularly housing for “empty-nesters” and retiring “baby-boomers.” The CCID should continue to promote development and redevelopment of mixed-use and residential housing to improve the jobs-to-housing balance within the area including a diversity of housing products/types.

# **BLUEPRINT CUMBERLAND II**

## **FINAL REPORT**

